

From: [EPD, Customer Services](#)
To: [TCCS_PC DA](#)
Subject: REFERRAL-TCCS-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Monday, 4 March 2019 8:52:00 AM
Attachments: [plans.obr](#)
[supporting_docs.obr](#)
[image001.png](#)

DEVELOPMENT APPLICATION NO: 201835108
BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

Description - **PROPOSAL FOR RAIL FREIGHT TERMINAL, FREIGHT TRANSPORT FACILITY AND ACCESS - construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), as well as access driveways and associated works.**

Pursuant to Section 148(1) of the *Planning and Development Act 2007* the ACT Planning and Land Authority requests that you consider the abovementioned development application and provide any written advice no later than **15 working days** after the date of this notice **(25/03/2019)**.

In accordance with Section 150 of the *Planning and Development Act 2007* If advice is not received within the prescribed time it will be taken that you have supported the application.

Please forward any written advice via email to Customer Services
EPDcustomerservices@act.gov.au

Please use the following format in the subject line of the email when providing advice:

COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TAMS-201401234-10/10 Dickson-01

Kind Regards

David

Phone 6207 1923

EPDCustomerServices@act.gov.au

www.act.gov.au/accesscbr

Access Canberra | ACT Government

16 Challis Street, Dickson | GPO Box 158 Canberra ACT 2601

cid:image004.png@01D3DE12.73C7D510



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From: [EPD, Customer Services](#)
To: [EPAPanningLiaison](#)
Subject: REFERRAL-EPA-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Monday, 4 March 2019 8:53:00 AM
Attachments: [plans.obr](#)
[supporting_docs.obr](#)
[image001.png](#)

DEVELOPMENT APPLICATION NO: 201835108
BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

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COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TAMS-201401234-10/10 Dickson-01

Kind Regards

David

Phone 6207 1923

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From: [EPD, Customer Services](#)
To: treeprotection-ac@act.gov.au
Subject: REFERRAL-TREE PROTECTION-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Monday, 4 March 2019 8:54:00 AM
Attachments: [plans.obr](#)
[supporting_docs.obr](#)
[image001.png](#)

DEVELOPMENT APPLICATION NO: 201835108
BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

Description - **PROPOSAL FOR RAIL FREIGHT TERMINAL, FREIGHT TRANSPORT FACILITY AND ACCESS - construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), as well as access driveways and associated works.**

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Please forward any written advice via email to Customer Services
EPDcustomerservices@act.gov.au

Please use the following format in the subject line of the email when providing advice:

COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TAMS-201401234-10/10 Dickson-01

Kind Regards

David

Phone 6207 1923

EPDCustomerServices@act.gov.au

www.act.gov.au/accesscbr

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From: [EPD, Customer Services](#)
To: [EPD DA Leasing referrals](#)
Subject: REFERRAL-DA LEASING-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Monday, 4 March 2019 8:55:00 AM
Attachments: [plans.obr](#)
[supporting_docs.obr](#)
[image001.png](#)

DEVELOPMENT APPLICATION NO: 201835108
BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

Description - **PROPOSAL FOR RAIL FREIGHT TERMINAL, FREIGHT TRANSPORT FACILITY AND ACCESS - construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), as well as access driveways and associated works.**

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EPDcustomerservices@act.gov.au

Please use the following format in the subject line of the email when providing advice:

COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TAMS-201401234-10/10 Dickson-01

Kind Regards

David

Phone 6207 1923

EPDCustomerServices@act.gov.au

www.act.gov.au/accesscbr

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cid:image004.png@01D3DE12.73C7D510



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From: [EPD, Customer Services](#)
To: devapp@actewagl.com.au; [Building Approvals - Icon Water](#)
Subject: REFERRAL-ACTEWAGL UTILITIES-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Monday, 4 March 2019 9:06:00 AM
Attachments: [image001.png](#)

PLEASE ACCEPT INVITATION TO JOIN OBJECTIVE CONNECT TO VIEW PLANS & SUPPORTING DOCS FOR THIS DEVELOPMENT APPLICATION

DEVELOPMENT APPLICATION NO: 201835108
BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

Description - **PROPOSAL FOR RAIL FREIGHT TERMINAL, FREIGHT TRANSPORT FACILITY AND ACCESS - construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), as well as access driveways and associated works.**

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COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TAMS-201401234-10/10 Dickson-01

Kind Regards

David

Phone 6207 1923

EPDCustomerServices@act.gov.au

www.act.gov.au/accesscbr

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ACT Government and deliver a more seamless experience.

From: [EPD, Customer Services](#)
To: worksapproval@nca.gov.au
Subject: REFERRAL-NCA-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Monday, 4 March 2019 9:07:00 AM
Attachments: [image001.png](#)

PLEASE ACCEPT INVITATION TO JOIN OBJECTIVE CONNECT TO VIEW PLANS & SUPPORTING DOCS FOR THIS DEVELOPMENT APPLICATION

DEVELOPMENT APPLICATION NO: 201835108
BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

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Please use the following format in the subject line of the email when providing advice:

COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TAMS-201401234-10/10 Dickson-01

Kind Regards

David

Phone 6207 1923

EPDCustomerServices@act.gov.au

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From: [Chowdhury, Abu Sayem](#)
To: [Bennett, JamesP](#)
Cc: [Riches, Dominic](#); [Joseph, Gabriel](#); [TCCS_PlaceCoord](#); [Davidson, Geoffrey](#)
Subject: RE: TCCS advice requested [SEC=UNCLASSIFIED]
Date: Monday, 13 May 2019 4:27:43 PM

Hi James

Thank you for your email.

I have had a chat with Dominic. As discussed I will prioritise all these submissions. See below expected completion dates in red.

Regards

Sayem

Abu Sayem Chowdhury | Senior Project Coordinator | M Engg | MIEAust

Phone: 02 6205 9091 | Email: abusayem.chowdhury@act.gov.au

Place Coordination and Planning | Transport Canberra and City Services Directorate | **ACT Government**

490 Northbourne Avenue, Dickson | GPO Box 158 Canberra ACT 2601

www.act.gov.au | www.tccs.act.gov.au |

From: Bennett, JamesP

Sent: Monday, 13 May 2019 1:37 PM

To: TCCS_PlaceCoord <TCCS.PlaceCoord@act.gov.au>; Chowdhury, Abu Sayem <AbuSayem.Chowdhury@act.gov.au>; Davidson, Geoffrey <Geoffrey.Davidson@act.gov.au>

Cc: Riches, Dominic <Dominic.Riches@act.gov.au>; Joseph, Gabriel <Gabriel.Joseph@act.gov.au>

Subject: TCCS advice requested

Hi Sayem/Geoffrey

I haven't been able to make contact with Gabriel as he appears to be away.

There are a number of applications where some advice from TCCS is required and we would like to progress the applications as soon as possible. The following applications require TCCS attention:



CRS DA - DA201835108

TCCS: Does not support the flood study and traffic report (same reasons as DA201834656) that was submitted as part of the application. TCCS are currently reviewing the revised traffic report and flood study that was submitted for DA201834656. Both studies will need to be submitted for this DA and reviewed by TCCS. Formal submission is required but has been delayed until we know the studies are sufficient (endorsed by TCCS - DA201834656).

Once a formal submission and referral is made then a quick turnaround is required. **ok**

Status: DA201835108 is currently waiting for further information (TCCS signed Authorisation letter). Please also specify whether a licence is required for works proposed on unleased land.

CRS DA - DA201834656

TCCS: Does not support the flood study and traffic report (same reasons as DA201835108). A revised flood study and traffic report has been submitted and referred to TCCS. Advice is due on 21 May 2019, however, TCCS have already reviewed previous studies. Advice is required urgently so that decisions can be made on DA201835108 and this DA. **Expected response on Friday 17 May**

Status: DA201834656 is currently waiting for advice from TCCS on the revised flood study and traffic report. Please also specify whether a licence is required for works

proposed on unleased land.



Could you please give me a call to discuss when you get a moment?

Thanks

James

James Bennett | Director, Impact Assessment and Business Improvement

Phone 02 6205 4877 | Planning Delivery Division

Environment, Planning and Sustainable Development Directorate | ACT Government

Dame Pattie Menzies House, 16 Challis Street, Dickson | GPO Box 1908 Canberra ACT 2601 | www.planning.act.gov.au

From: [Riches, Dominic](#)
To: [Perkins, Jessica](#)
Cc: [Sargent, Narelle](#); [Bennett, JamesP](#)
Subject: RE: REFERRAL-EPA-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Monday, 20 May 2019 10:14:00 AM
Attachments: [image002.png](#)
[image003.png](#)
[plans.obr](#)
[supporting_docs.obr](#)

Good morning Jessica

I am currently assisting the merit team with the assessment of the above DA and would just like to clarify the advice below. EPA has not supported the noise management plan/application as the plans do not depict the recommended acoustic wall. The advice states below that a certifier may ensure the wall is constructed. However, at this stage, a condition could be imposed to ensure that the wall is included in the final plans. The condition could be as follows:

Prior to releasing/approving the plans

Revised plans must be submitted incorporating the acoustic fence as recommended in the noise management plan (*Rudds Consulting, February 2018*) which was submitted with the application.

Can you please let me know, with the above condition imposed, whether the EPA can endorse the application.

Regards

Dominic Riches | Impact Assessment and Business Improvement

Phone 02 6205 1834

Planning Delivery

From: Sargent, Narelle

Sent: Monday, 25 March 2019 4:33 PM

To: EPD, Customer Services <EPDCustomerServices@act.gov.au>

Cc: EPAPanningLiaison <EPAPanningLiaison@act.gov.au>; Power, David <DAVID.POWER@act.gov.au>

Subject: REFERRAL-EPA-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]

Dear Environment Planning & Sustainable Development Directorate
DA 201835108

BLOCK: 11,9,11	SECTION: 8,8,47	DIVISION: Fyshwick
No comments		
Recommend Conditions of Approval		
Advice for the applicant		
Recommend Lease Conditions		
Recommend Not Supported	X	
Further Information/amendments Required		

The Environment Protection Authority (EPA) **does not support** the development application for the construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), access driveways and associated works in its current form.

The noise management plan submitted with the development application recommends that an acoustic wall be constructed along the southern boundary of the site; however, the details of this wall have not been incorporated into the submitted plans. Because there are no mechanisms in place to ensure that recommendations in noise management plans are incorporated into the construction of the development, the EPA is unable to support the DA until amended plans showing the recommended acoustic wall are submitted.

If the EPA could be assured that a private certifier were able to assess conditions of consent prior

to the issue of a Certificate of Occupancy, the following condition may be suitable:

- *The Noise Protection Wall recommended by Rudds Consulting Engineers in the submitted Noise Management Plan (Rudds, February 2018) must be constructed prior to the issue of a Certificate of Occupation.*

Further to the above comments, the following conditions and advice regarding contaminated land are provided:

Conditions:

- The sites must be assessed and remediated by a suitably qualified environmental consultant and these works independently audited by an EPA approved contaminated land auditor prior to any change of use; The auditor's findings into the site's suitability from a contamination perspective for its proposed and permitted uses under the Territory Plan must be reviewed and endorsed by the EPA prior to the commencement of development works and prior to the site being used for other purposes;
- All soil subject to disposal from the site must be assessed in accordance with EPA Information Sheet 4 - Requirements for the reuse and disposal of contaminated soil in the ACT; and
- No soil is to be disposed from site without EPA approval.

Advice:

- EPA records indicate that Blocks 9 and 11 were formerly occupied by the Shell Canberra depot and associated raiing sidings. Hydrocarbon related impacts to soil and groundwater have been identified at these sites;
- Block 11 Section 47 is currently occupied by the ACT to NSW rail corridor with potential land contamination issues associated with this activity.

For further information please contact the Environment Protection Authority Planning Liaison on 02 6207 5642.

Yours sincerely

Narelle Sargent

Environment Protection Authority

Narelle Sargent | the Environment Protection Authority

Office of the Environment Protection Authority (EPA)

Access Canberra | Chief Minister Treasury and Economic Development Directorate | ACT

Phone: 02 6207 5782 | Mobile [REDACTED] | Email: narelle.sargent@act.gov.au

470 Northbourne Avenue, Dickson | GPO Box 158 Canberra ACT 2601 | www.act.gov.au/accessCBR

cid:image007.png@01D3AFF2.FEECBAF0



From: EPD, Customer Services

Sent: Monday, 4 March 2019 8:54 AM

To: EPAPlanningLiaison <EPAPlanningLiaison@act.gov.au>

Subject: REFERRAL-EPA-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]

DEVELOPMENT APPLICATION NO: 201835108

BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

Description - **PROPOSAL FOR RAIL FREIGHT TERMINAL, FREIGHT TRANSPORT FACILITY AND ACCESS - construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), as well as access driveways and associated works.**

Pursuant to Section 148(1) of the *Planning and Development Act 2007* the ACT Planning and Land Authority requests that you consider the abovementioned development application and provide any written advice no later than **15**

working days after the date of this notice **(25/03/2019)**.

In accordance with Section 150 of the *Planning and Development Act 2007* If advice is not received within the prescribed time it will be taken that you have supported the application.

Please forward any written advice via email to Customer Services

EPDcustomerservices@act.gov.au

Please use the following format in the subject line of the email when providing advice:

COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TAMS-201401234-10/10 Dickson-01

Kind Regards

David

Phone 6207 1923

EPDCustomerServices@act.gov.au

www.act.gov.au/accesscbr

Access Canberra | ACT Government

16 Challis Street, Dickson | GPO Box 158 Canberra ACT 2601

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From: [Riches, Dominic](#)
To: [TCCS_PlaceCoord](#)
Cc: [Joseph, Gabriel](#); [Chowdhury, Abu Sayem](#); [Bennett, JamesP](#)
Subject: FW: REFERRAL-TCCS-201835108-11,9,11/8,8,47 FYSHWICK-01 [SEC=UNCLASSIFIED]
Date: Tuesday, 21 May 2019 4:40:00 PM
Attachments: [APP-201835108-S141B-01.pdf](#)
[AUTHORISATION-201835108-S141B-01.pdf](#)
[STORMWATERREPORT-201835108-S141B-01.pdf](#)
[TRAFFICREPORT-201835108-S141B-01.pdf](#)

DEVELOPMENT APPLICATION NO: 201835108/S141B
BLOCK: 11,9,11 **SECTION:** 8,8,47 **DIVISION:** FYSHWICK

Description - **PROPOSAL FOR RAIL FREIGHT TERMINAL, FREIGHT TRANSPORT FACILITY AND ACCESS - construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), as well as access driveways and associated works.**

Further information has been submitted under s 141 of the *Planning and Development Act 2007*. Pursuant to Section 148(1) of the *Planning and Development Act 2007* the ACT Planning and Land Authority requests that you consider the abovementioned development application and provide any written advice no later than 15 working days after the date of this notice (**ASAP**).

In accordance with Section 150 of the *Planning and Development Act 2007*. If advice is not received within the prescribed time it will be taken that you have supported the application.

Please forward any written advice via email to Customer Services
EPDcustomerservices@act.gov.au

Please use the following format in the subject line of the email when providing advice:
 COMM-Agency Name-20140XXXX-Block XX Section XX SuburbXXXXX-01

Example: COMM-TCCS-201401234-10/10 Dickson-01

Regards

Dominic Riches | Assistant Director, Impact Assessment and Business Improvement
Phone 02 6205 1834 | email dominic.riches@act.gov.au
Planning Delivery Division | Environment, Planning and Sustainable Development | **ACT Government**
 Dame Pattie Menzies House, 16 Challis Street, Dickson | GPO Box 1908 Canberra ACT 2601 | www.planning.act.gov.au

Dear Mr/s Conant

In accordance with section S141 of the *Planning and Development Act 2007*, before this application can be determined, the Directorate requires the information below to be lodged by the period stated:

DA Number: 201835108

Block: 11, Section: 47

Suburb: FYSHWICK, District: CANBERRA CENTRAL

Site Details: Block: 11, Section: 8
Suburb: FYSHWICK, District: CANBERRA CENTRAL

Block: 9, Section: 8

Suburb: FYSHWICK, District: CANBERRA CENTRAL

Applicant Name: Emma Conant

Information Required: Further information is required for this development application. Please provide a current letter of authorisation (form 4) for block 11 section 47 Fyshwick from the land custodian (TCCS). Form 4 will need to be completed using the up-to-date version, as per the Legislation Register (<https://www.legislation.act.gov.au/af/2017-39/>).

Further Information due date: 7/05/2019

If you cannot provide your response within the required timeframe, it is recommended you write to the Directorate prior to the expiration of the period stated above and seek an extension of the prescribed period for providing the information.

Please note, Section 141 (4) of the *Planning and Development Act 2007* provides that only one such extension may be granted.

If there is any change to the proposal as a result of responding to the further information request, the amended proposal is required to be submitted pursuant to S144 of the *Planning and Development Act 2007*. Please note the amended proposal will need to go through assessment and the due date will be extended as a consequence of submission pursuant to S144 of the *Planning and Development Act 2007*.

You are advised that if some or all of the information has not been provided in accordance with this request, Section 142 of the *Planning and Development Act 2007* provides that the Directorate may refuse the application under Section 162.

This email was automatically generated - **please do not respond**. If you need to contact the Environment, Planning and Sustainable Development Directorate in

relation to this proposal please contact Customer Services on (02) 6207 1923 or email EPDcustomerservices@act.gov.au

Customer
Services
Environment,
Planning and
Sustainable
Development
Directorate

Dear Mr/s Conant

In accordance with section S141 of the *Planning and Development Act 2007*, before this application can be determined, the Directorate requires the information below to be lodged by the period stated:

DA Number: 201835108

Block: 11, Section: 47

Suburb: FYSHWICK, District: CANBERRA CENTRAL

Site Details:

Block: 11, Section: 8

Suburb: FYSHWICK, District: CANBERRA CENTRAL

Block: 9, Section: 8

Suburb: FYSHWICK, District: CANBERRA CENTRAL

Applicant Name:

Emma Conant

Information Required:

Further information is required for the above development application. Please provide (1) An authorisation letter dated by the applicant. (2) A revised traffic report and revised flood study in accordance with Transport Canberra and City Services (TCCS) requirements.

Further Information due date:

19/06/2019

If you cannot provide your response within the required timeframe, it is recommended you write to the Directorate prior to the expiration of the period stated above and seek an extension of the prescribed period for providing the information.

Please note, Section 141 (4) of the *Planning and Development Act 2007* provides that only one such extension may be granted.

If there is any change to the proposal as a result of responding to the further information request, the amended proposal is required to be submitted pursuant to S144 of the *Planning and Development Act 2007*. Please note the amended proposal will need to go through assessment and the due date will be extended as a consequence of submission pursuant to S144 of the *Planning and Development Act 2007*.

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email EPDcustomerservices@act.gov.au

Customer
Services
Environment,
Planning and
Sustainable
Development
Directorate

From: [Ligal, Bibek](#)
To: [DiCampli, Nicki](#)
Subject: RE: Development Application 201835108 [SEC=UNCLASSIFIED]
Date: Tuesday, 29 October 2019 5:11:46 PM
Attachments: [image001.png](#)
[image002.png](#)

Thank you Nicki,

I have contacted the consultants and they will be lodging another application directly to us for the other DA. I will provide an Entity Advice to the consultant who should be able to then forward it on to yourself.

Regards,

Bibek Ligal
 Building Approval and Network Protection Officer
 Urban Development Services



Icon Water
 GPO Box 366 Canberra ACT 2601
 T [REDACTED] M [REDACTED]
iconwater.com.au | [Twitter](#) | [YouTube](#) | [LinkedIn](#)

cid:image002.png@01D1FEBA.2AFB27D0

Important notice for Canberra Developers: The transition period for the Water and Sewerage Capital Contributions Code (WSCCC) charge ended on 30 June 2019. From 1 July 2019 all developments fitting the criteria explained in the code will attract a WSCCC charge. For further information, please visit our website at <http://www.iconwater.com.au/capitalcontributions>

From: DiCampli, Nicki <Nicki.DiCampli@act.gov.au>
Sent: Tuesday, 29 October 2019 4:35 PM
To: Ligal, Bibek [REDACTED] >
Subject: Development Application 201835108 [SEC=UNCLASSIFIED]

CAUTION: This email originated from outside of the organisation. Do not click links or open attachments unless you recognise the sender and are expecting the content or attachment from the sender

Hi Bibek

Thank you for talking to me earlier this afternoon in regard to the Fyshwick proposals.

I am just letting you know I won't be in the office tomorrow if you have any further information in relation to the Icon water clearance required for DA 201835108 (copy of decision attached).

If you can let me know what needs to happen and/or if I need to call you to discuss.

Thanks and regards

Nicki Di-Campli

Nicki Di-Campli | Assessment Officer

Phone 02 6207 1963 |

Development Assessment | Environment, Planning and Sustainable Development Directorate | ACT Government
 Dame Pattie Menzies House, Challis Street, Dickson | GPO Box 1908 Canberra ACT 2601 | www.planning.act.gov.au

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From: [Riches, Dominic](#)
To: [Emily Leemhuis](#)
Cc: [DiCampfi, Nicki](#)
Subject: RE: CRS DA's (201834656 & 201835108)
Date: Tuesday, 29 October 2019 5:00:12 PM
Attachments: [image001.png](#)
[image002.jpg](#)

UNOFFICIAL

Hi Emily

Stage 6 has reviewed the documentation and has discussed with Icon Water the above DAs.

Icon Water have informed us that the Statement of Conditional Acceptance (application No: 179611) only applies to DA201834656. Icon Water have stated that a separate application is required, however, if provided quickly, could be finalised by the end of the week. Stage 6 have also confirmed that once the statement of conditional acceptance is provided then they can act quickly to finalise.

Bibek Ligal (Icon Water) will contact you to resolve.

Regards

Dominic Riches | Post-DA Review
Phone 02 6205 1834
Planning Delivery

From: Riches, Dominic
Sent: Monday, 28 October 2019 3:37 PM
To: Emily Leemhuis [REDACTED]
Subject: RE: CRS DA's (201834656 & 201835108)

UNOFFICIAL

Thanks Emily

I have discussed with the stage 6 team who said they should be able to send it out pretty quickly.

Regards

Dominic Riches | Post-DA Review
Phone 02 6205 1834
Planning Delivery

From: Emily Leemhuis [REDACTED]

Sent: Monday, 28 October 2019 3:26 PM
To: Riches, Dominic <Dominic.Riches@act.gov.au>
Subject: CRS DA's (201834656 & 201835108)

Hi Dominic,

Following on from our discussion the other week re additional information required prior to release of stamped plans, please see attached the following documents for your consideration:

- Updated plan stating that the acoustic fence is in accordance with the Rudds plan
- Icon Water Conditional Acceptance (both DA)
- Amended lighting plan with correct AS references

Let me know if there's anything further outstanding.

Best Regards,

Emily Leemhuis

Senior Urban Planner

Purdon Planning Pty Ltd

Unit 4, Cooyong Centre

1 Torrens Street

Braddon ACT 2612

T: 02 6257 1511

www.purdon.com.au



From: [Emily Leemhuis](#)
To: [Riches, Dominic](#)
Cc: [DiCampli, Nicki](#)
Subject: RE: CRS DA's (201834656 & 201835108)
Date: Monday, 4 November 2019 10:04:29 AM
Attachments: [image001.png](#)
[image002.jpg](#)
[31102019112813-0001.pdf](#)

Hi Dominic,

Further to the below, see attached Evo compliance.

Regards,

Emily Leemhuis Senior Urban Planner	Purdon Planning Pty Ltd Unit 4, Cooyong Centre 1 Torrens Street Braddon ACT 2612	T: 02 6257 1511 www.purdon.com.au
------------------------------------------------------	-------------------------------------------------------------------------------------------	-----------------------------------------------------------------------------



From: Emily Leemhuis
Sent: Monday, 4 November 2019 9:52 AM
To: Riches, Dominic <Dominic.Riches@act.gov.au>
Cc: DiCampli, Nicki <Nicki.DiCampli@act.gov.au>
Subject: RE: CRS DA's (201834656 & 201835108)

Hi Dominic,

See attached Icon approvals for DA201835108.

Regards,

Emily Leemhuis Senior Urban Planner	Purdon Planning Pty Ltd Unit 4, Cooyong Centre 1 Torrens Street Braddon ACT 2612	T: 02 6257 1511 www.purdon.com.au
------------------------------------------------------	-------------------------------------------------------------------------------------------	-----------------------------------------------------------------------------



From: Riches, Dominic <Dominic.Riches@act.gov.au>
Sent: Tuesday, 29 October 2019 5:00 PM
To: Emily Leemhuis [REDACTED]

Cc: DiCampli, Nicki <Nicki.DiCampli@act.gov.au>

Subject: RE: CRS DA's (201834656 & 201835108)

UNOFFICIAL

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Gas Networks Statement of CONDITIONAL COMPLIANCE

For Non-residential and High Rise Residential

Application N°:	<u>179970</u>	Drawings in set	<u>6</u>
Block:	<u>11</u>	Section:	<u>47</u>
Suburb:	<u>FISHWICK</u>		

This application has been assessed against legislation protecting Evoenergy's gas infrastructure and access to it.

This application is approved subject to compliance with the following conditions:

- The location and area allocated for gas regulating and metering equipment is to comply with Evoenergy Gas Service and Installation Rules. The latest version of these rules can be downloaded from:
<http://www.actewagl.com.au/About-us/The-ActewAGL-network/Natural-gas-network.aspx>
- Development is to comply with minimum separation requirements to underground assets
 - 300mm minimum clearance from major plastic and steel gas mains and steel gas services
 - 150mm minimum clearance from other plastic gas mains and services
- A metering equipment upgrade may be required. A licensed gas fitter should verify loads and metering equipment capacities.
- If a meter relocation or service pipe relocation is required in order to comply with Evoenergy standards, please contact your gas retailer and book a meter relocation. Only people accredited by Evoenergy can carry out this work.
- Other:

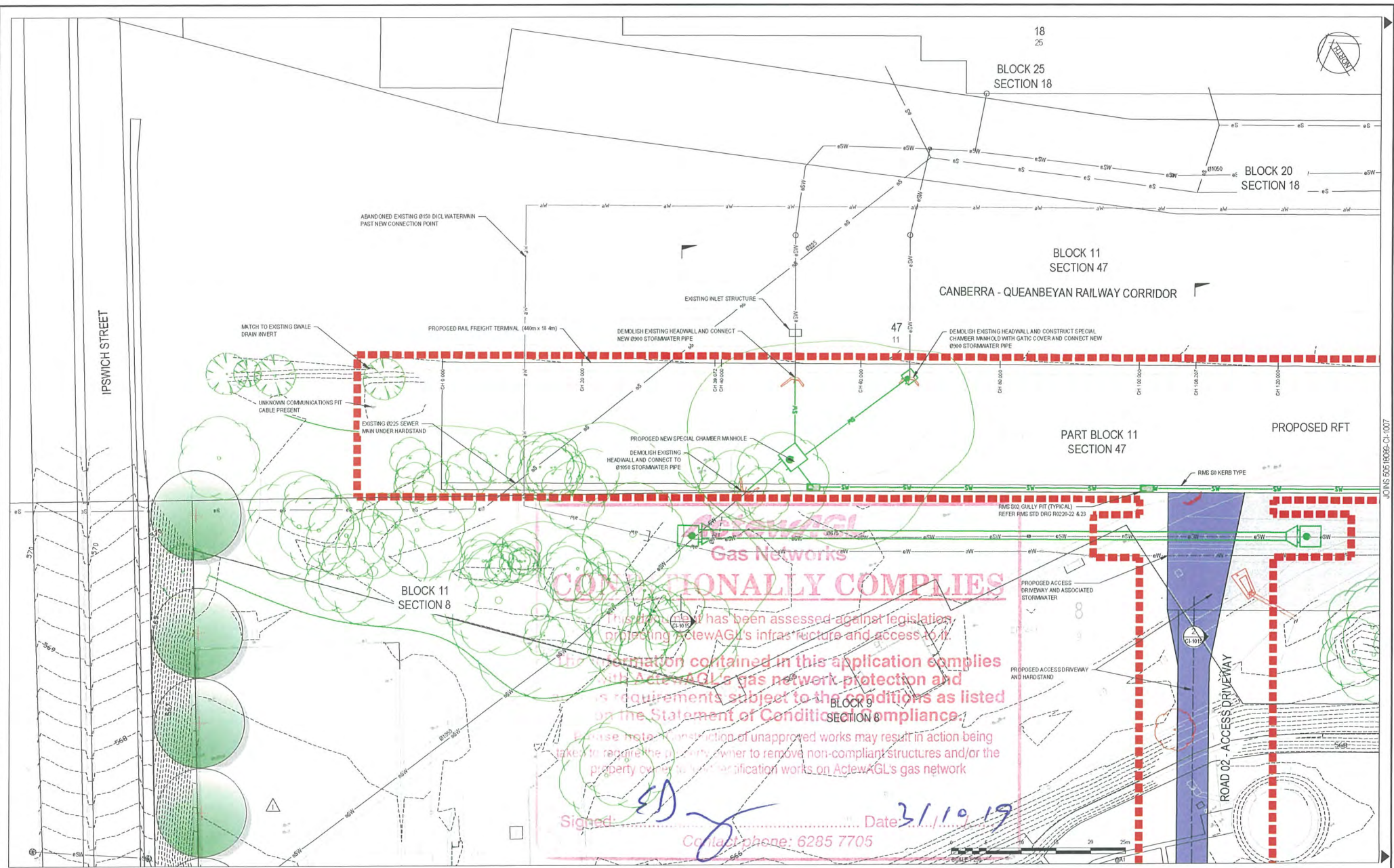
Please note:

- WARNING: Evoenergy underground gas pipes may be in or adjacent to this block. Evoenergy Asset Location Advice may be required. Call 1100.
- Development and Building Applications will need to include any proposed Evoenergy works
- If Evoenergy approval conditions are not met, a breach of the law may result.
- Separate applications are required for water & sewerage, electricity and communication network services.
- Construction of unapproved works may result in action being taken to require the property owner to remove non-compliant structures and/or the property owner to fund rectification works on Evoenergy's gas network
- Any attached reticulation or servicing plan is preliminary only. Contact Evoenergy for final plans prior to the commencement of any construction activity

Signed..........Date 31/10/19.....

For further information please phone Steve Donnelly – Jemena 6192 6270

DATE PLOTTED: 20 October 2019 4:00 PM BY: GOLAMHOSSEIN



Rev	Date	Description	Des	Verif	App'd
1	20/06/2019	RE SUBMISSIONS	GH	AS	GZ
H	14/03/2019	RE SUBMISSIONS	PDJ	TL	GZ
G	19/12/2018	RE SUBMISSIONS	PDJ	TL	GZ
F	18/12/2018	RE SUBMISSIONS	PDJ	TL	GZ
E	11/12/2018	RE SUBMISSIONS	PDJ	TL	GZ
D	24/09/2018	DA SUBMISSIONS - COMMENTS	PDJ	TL	GZ
C	14/06/2018	REVISED DA SUBMISSION	PDJ	TL	GZ
B	22/05/2018	DA SUBMISSION	PDJ	TL	GZ
A	11/05/2018	CLIENT REVIEW	PDJ	TL	GZ



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Drawn	Date	Client
PDJ	14/06/2019	CAPTIAL RECYCLING SOLUTIONS PTY LTD
Checked	Date	Project
TL	14/06/2019	PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY)
Designed	Date	EG AND B11 S8 FYSHWICK (ACCESS AND SW CONNECTION)
PDJ/TL	14/06/2019	
Verified	Date	TITLE
GZ	14/06/2019	GENERAL ARRANGEMENT PLAN
Approved	Date	SHEET 1 OF 3
	14/06/2019	

Status	Scale	Size
FOR APPROVAL	1:250	A1
NOT TO BE USED FOR CONSTRUCTION PURPOSES		
Datum	Scale	Size
AHD	1:250	A1
Drawing Number	Revision	
50518089-CI-1006	I	

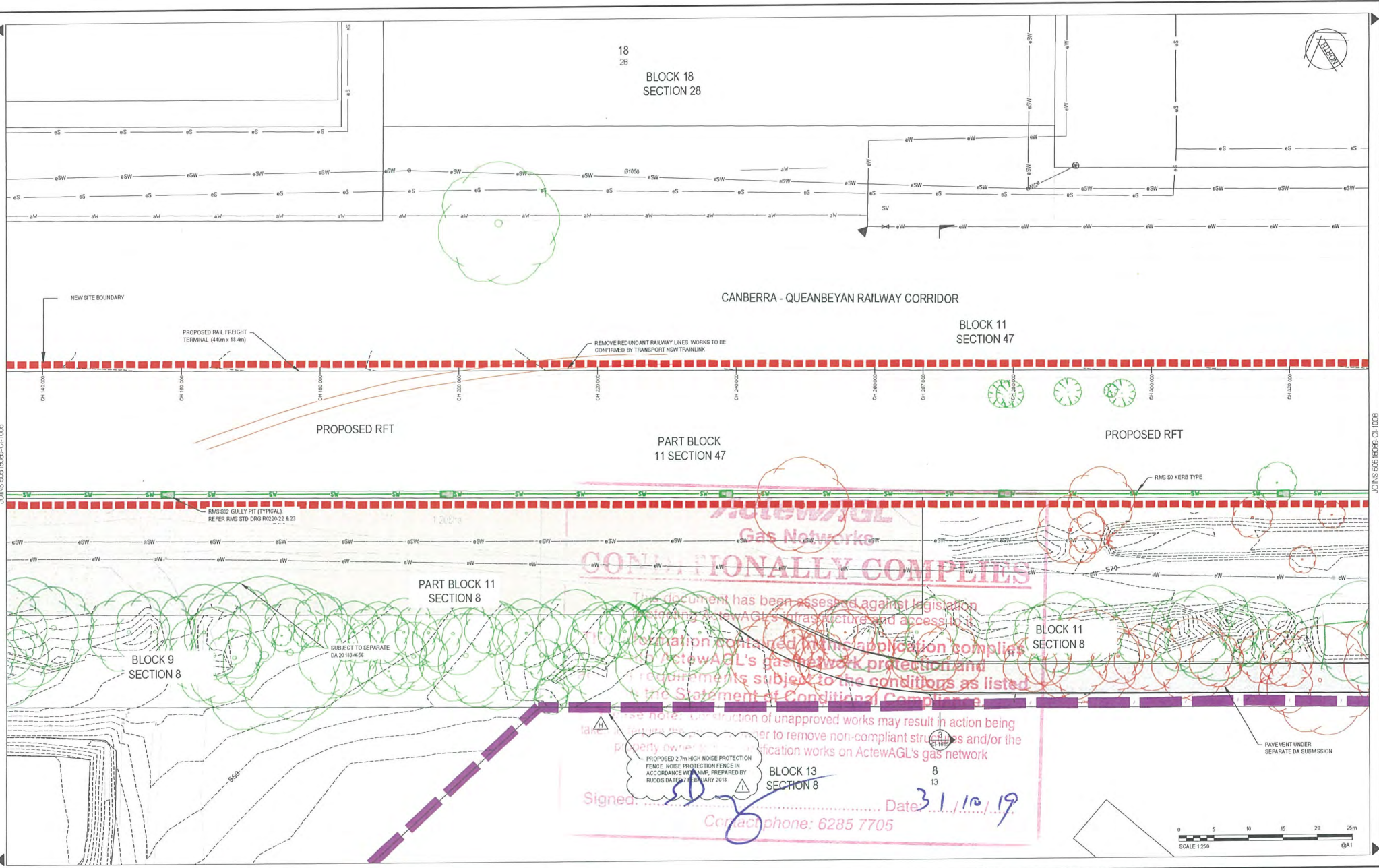
XREF's: x_Existing services, x_TOD Background, x_Survey, x_Gummy Coats, x_TREES, x_Bank-2 CAD File, N_Planetree/05/05/19/19193_CRS/RAIL/SDING/B11_S47_FYSHWICK/Drawings/Block 11/Section 47/0518089-CI-1006-CA.dwg

JOINS 50518089-CI-1007

DATE PLOTTED: 20 October 2018 4:45 PM BY: COLMANJCS@AN

JOINS 505 18089-CI-1006

REF: L:\Working\arcgis_110\Background_Survey_Survey_Coort_L_TREES_18089-1006.dwg
 C:\File\N:\Projects\505\FYSHWICK\B11_S47\FYSHWICK\Background\B11_S47.dwg
 JOINS 505 18089-CI-1006.dwg



ACTEWAGL Gas Networks
CONDITIONALLY COMPLIES

This document has been assessed against legislation relating to ActewAGL's infrastructure and access to it. The information contained herein complies with ActewAGL's gas network protection and requirements subject to the conditions as listed in the Statement of Conditional Compliance.

Please note, construction of unapproved works may result in action being taken to require the owner to remove non-compliant structures and/or the property owner to undertake remediation works on ActewAGL's gas network.

Rev	Date	Description	Des	Verd	Appd
T	24/10/2018	NOTES UPDATED	GH	AS	GZ
H	23/06/2018	RE SUBMISSIONS	GH	AS	GZ
G	19/12/2018	RE SUBMISSIONS	POJ	TL	GZ
F	18/12/2018	RE SUBMISSIONS	POJ	TL	GZ
E	11/12/2018	RE SUBMISSIONS	POJ	TL	GZ
D	24/07/2018	DA SUBMISSIONS - COMMENTS	POJ	TL	GZ
C	14/05/2018	REVISED DA SUBMISSION	POJ	TL	GZ
B	23/05/2018	DA SUBMISSION	POJ	TL	GZ
A	11/05/2018	CLIENT REVIEW	POJ	TL	GZ



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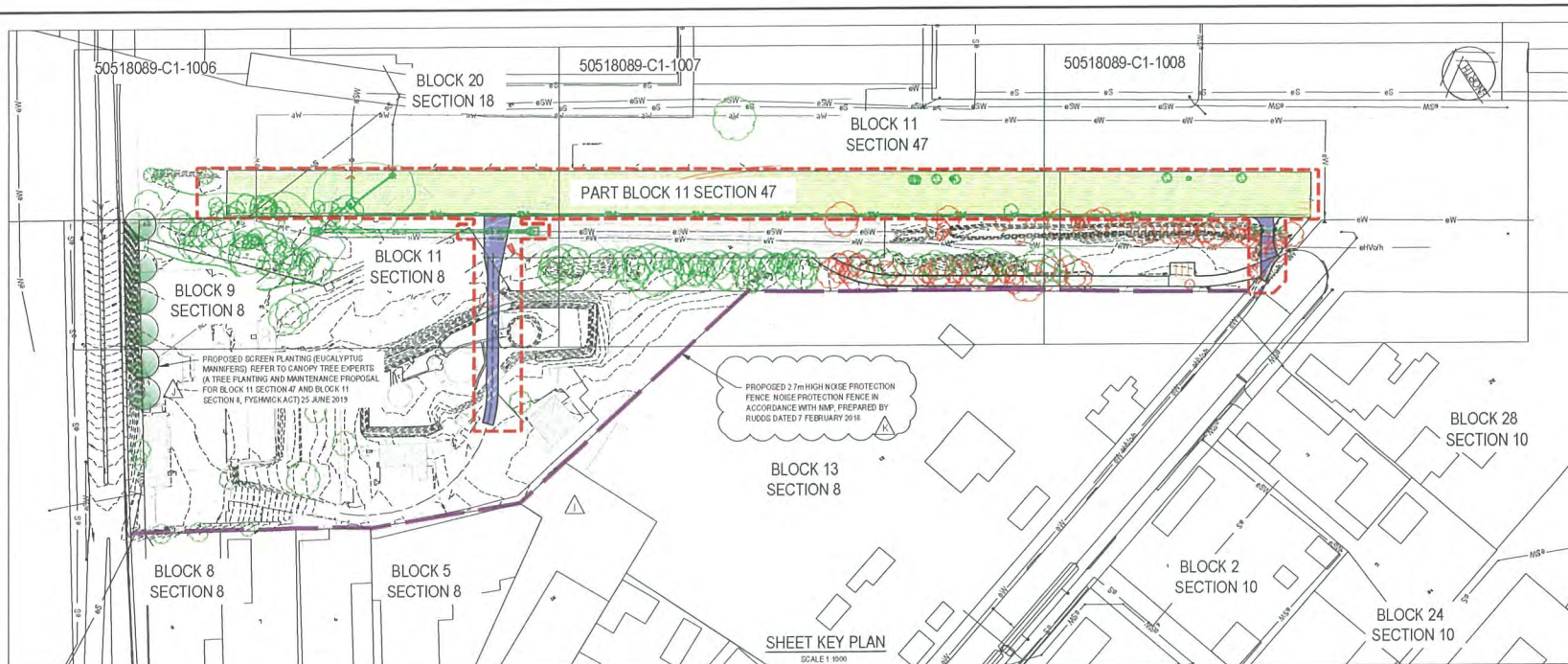
Cardno (NSWACT) Pty Ltd | ABN 95 001 145 035
 Level 2, 14 Kennard Street
 Sydney NSW ACT 2609
 Tel 02 6112 4500 Fax 02 6112 4599
 Web www.cardno.com.au

Drawn	Date
POJ	14/06/2018
Checked	
TL	14/06/2018
Designed	
POJ/TL	14/06/2018
Worked	
GZ	14/06/2018
Approved	
JPC	14/06/2018

Client: CAPTIAL RECYCLING SOLUTIONS PTY LTD
 Project: PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY) E9 AND B11 S9 FYSHWICK (ACCESS AND SW CONNECTION)
 Title: GENERAL ARRANGEMENT PLAN SHEET 2 OF 3

FOR APPROVAL			
NOT TO BE USED FOR CONSTRUCTION PURPOSES			
Status	Scale	Size	Revision
AHD	1:250	A1	
Drawing Number	505 18089-CI-1007		Revision
			1

DATE PLOTTED: 31 October 2014 09:50:01 USER: G:\ADMIN\HCS\SA



LEGEND

- STAGE BOUNDARY
- PROPOSED 2.7m HIGH NOISE PROTECTION FENCE
- PROPOSED RAIL FREIGHT TERMINAL
- PROPOSED ACCESS DRIVEWAYS (BLOCK 9 & 11, SECTION 8)
- PAVEMENTS SUBJECT TO SEPARATE DA SUBMISSION (S010-04-05)
- PROPOSED SCREEN PLANTING (EUCALYPTUS MANIFERS)
- EXISTING TREE TO BE RETAINED
- EXISTING TREE TO BE REMOVED

PROPOSED SERVICES

- STORMWATER
- SWALE / CUT OFF DRAIN
- SAWTOOTH AND/OR MATCH TO EXISTING SMOOTHLY

EXISTING SERVICES

- STORMWATER MAINHOLE
- WATER
- SEWER MAINHOLE
- COMMUNICATIONS - TELSTRA
- COMMUNICATIONS - NEXTGEN
- GAS
- ELECTRICAL - OVERHEAD HIGH VOLTAGE
- ELECTRICAL - STREET LIGHT
- EXISTING STREET LIGHT
- SERVICE TO BE REMOVED

STORMWATER STRUCTURE NOTATION

- PS1 DENOTES PLANTATION SUMP REFER STD DRG No ST-0013 FOR DETAILS
- GS GRATED SUMP
- CSM1 SPECIAL CHAMBERED MAINHOLE
- DS DENOTES STORMWATER STRUCTURE No DENOTES STORMWATER LINE No
- AS STRUCTURE COVERS SHALL BE DESIGNED AND INSTALLED IN ACCORDANCE WITH AS3936

SUBSOIL DRAINAGE NOTES

- ALL SUBSOIL DRAINING SHALL BE INSTALLED IN ACCORDANCE WITH THE SPECIFICATION
- ALL SUBSOIL DRAINAGE SHALL BE 8000 CLASS S810 IN ACCORDANCE WITH AS 2431.1
- 8000 uPVC CLASS S810 SHALL BE USED WHERE SUBSOIL PIPE CROSSES ROAD PAVEMENTS, MN GRADE 1.0%
- CONNECTION OF SUBSOIL DRAINING TO SUBSOIL DRAINING SHALL BE WITH MANUFACTURED FITTINGS AND DUCT TAPED TO SECURE PIPE TO FITTING, INCLUDING JOINING UNTO EXISTING SSO
- GRADE SUBSOIL AT MINIMUM 1% FROM OUTLET POINT
- COMPACTION OF 7mm CRUSHED ROCK FILTER MATERIAL TO BE:
 - A) IN LANDSCAPED AREAS - COMPACTED TO DENSITY INDEX 70% IN PAVED AREAS
 - B) IN TOP 300mm BELOW SUBGRADE - COMPACTED TO DENSITY INDEX 90%
 - C) IN LOWER LAYERS - COMPACTED TO DENSITY INDEX 80%

REINSTATEMENT OF TRENCHES

- ALL TRENCHES SHALL BE BACKFILLED AND COMPACTED IN ACCORDANCE WITH THE DRAWINGS AND SPECIFICATION. SURFACE FINISHES SHALL BE REINSTATED TO MATCH WITH EXISTING ADJUTING SURFACES WITHOUT STEPPING AND LIPPING.

LANDSCAPING

- ALL WORKS IN THE WORKS THE NECESSARY SET DOWNS, LEVELS AND GRADES FOR LANDSCAPE FINISHES TO BE APPLIED, REFER TO LANDSCAPE ARCHITECT'S DRAWINGS FOR DETAILS OF FINISHES

REINFORCEMENT

- ALL REINFORCEMENT SHALL HAVE 580MPa TENSILE STRENGTH TO THE SIZES/TYPE SHOWN ON THE DRAWINGS.

SCALE 1:1000

GENERAL NOTES

- ALL CONSTRUCTION WORK SHALL BE CARRIED OUT IN ACCORDANCE WITH THE SPECIFICATION AND WHERE WORKS OCCURS ON TERRITORY LAND IN ACCORDANCE WITH THE ACT STANDARD SPECIFICATION FOR URBAN INFRASTRUCTURE.
- ALL CONSTRUCTION WORK SHALL BE CO-ORDINATED WITH ADJACENT CONTRACTS.
- THE LOCATION OF EXISTING SERVICES ARE SHOWN IN THEIR APPROXIMATE LOCATION ONLY. PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ON SITE, THE CONTRACTOR SHALL CONTACT THE RELEVANT AUTHORITIES AND/OR ASSET OWNERS/STAKEHOLDERS AND VERIFY THE LOCATION OF ALL UNDERGROUND SERVICES AND OBTAIN NECESSARY CLEARANCES.
- THE CONTRACTOR SHALL ENSURE THAT THE INSTALLATION OF UNDERGROUND SERVICES ARE CO-ORDINATED AND PLANNED PRIOR TO COMMENCING INSTALLATION. NO SEPARATIONS OR COMMUNICATED ALIGNMENT ZONES IS PERMITTED.
- THE CONTRACTOR SHALL GIVE ACT ROADS SUFFICIENT NOTIFICATION TO ALLOW EXISTING ROAD PAVEMENT MAINTENANCE TO OCCUR WHERE THE EXISTING PAVEMENT HAS FAILED AND WILL NOT ALLOW THE SATISFACTORY MATCHING OF THE NEW PAVEMENT.
- SURFACES WHICH LIE OUTSIDE THE GENERAL LIMITS OF LANDSCAPING AND RESTORATION WHICH ARE DISTURBED DURING THE CONSTRUCTION OF THE WORKS SHALL BE RESTORED BY THE CONTRACTOR. AT THE CONTRACTOR'S EXPENSE, TO AT LEAST THEIR PRE-CONSTRUCTION CONDITION. THESE SURFACES INCLUDE BUT ARE NOT NECESSARILY LIMITED TO, PAVEMENTS, PAVING, GRASSING, ETC.
- ALL DESIGN SUBGRADE ORC VALUES FOR PAVEMENTS AND PARKING MUST BE CONFIRMED BY THE CONTRACTOR PRIOR TO COMMENCEMENT OF CONSTRUCTION. RESULTS OF ADDITIONAL SUBGRADE TESTS MUST BE MADE AVAILABLE TO THE SUPERINTENDENT WITHIN 24 HOURS OF RECEIPT IF THE RESULTS ARE LESS THAN THE CURRENT DESIGN. OTHER A PAVEMENT RE-DESIGN WILL BE REQUIRED BEFORE PROCEEDING.
- ALL SERVICE PIT COVERS LOCATED IN FOOTPATHS AND ROADWAYS SHALL BE CLASS 5 GATIC COVERS UNO AND BE FILLED WITH THE SAME MATERIAL AS SURROUNDING MATERIAL.
- ALL SERVICE PIT COVERS LOCATED IN FOOTPATHS AND SHARED ZONES OR VERGES SHALL BE ORIENTATED PARALLEL OR PERPENDICULAR TO THE PAVED BOUNDARY.
- ALL GRATED SUMPS SHALL BE FITTED WITH HEEL GUARD COVERS.
- ALL NEW SERVICES / UTILITIES INFRASTRUCTURE TO BE INSTALLED WITH NECESSARY UNDERGROUND MARKING TAPE IN ACCORDANCE WITH AS 2648.

STORMWATER PIPE NOTES

- Ø100 AND Ø150 uPVC CLASS S810 AND S816, RESPECTIVELY TO AS1260 PARTS 1 TO 4 WITH SOLVENT WELDED JOINTS EXCEPT WHERE RUBBER RING JOINTS ARE REQUIRED ADJACENT TO SUMPS TO ALLOW MOVEMENT UNO.
- ALL uPVC PIPES SHALL BE INSTALLED IN ACCORDANCE WITH AS2032 AND THE SPECIFICATION.
- INTERNAL STORMWATER DRAINAGE SERVICES THESE SHALL HAVE A MINIMUM GRADE OF 1% AND BE END CAPPED.
- INSTANT STRIP PACKEDHEAD AND VIBRATED VERTICALLY CAST CONCRETE PIPES AND SIMILARLY MANUFACTURED CONCRETE PIPES SHALL NOT BE USED.
- ALL JUNCTIONS SHALL BE MADE WITH MANUFACTURED FITTINGS.
- ALL BACKFILLING UNDER ROAD AND CARPARK PAVEMENTS AND PAVING SHALL BE IN ACCORDANCE WITH THE SPECIFICATION AND THE DRAWINGS.
- CONNECTION TO STORMWATER PIPES WITH A BRANCH CONNECTION OF Ø100 OR Ø150 uPVC SHALL BE WITH FRC SADDLE ADAPTOR OR A uPVC BRANCH. REFER TCCS STD DRG No ST-0001.
- MINIMUM GRADES ON Ø100 AND Ø150 uPVC STORMWATER SHALL BE 1% UNLESS NOTED OTHERWISE.
- ALL PIPE LENGTHS, PIPE INVERT LEVELS AND GRADES ARE MEASURED AT CENTRE OF STORMWATER STRUCTURE.
- STEP IRONS ARE REQUIRED IN SW SUMPS AND MN HOLES THAT ARE DEEPER THAN 900mm REFER TCCS DESIGN STANDARDS.
- SCOUR STOPS FOR STORMWATER PIPES SHALL BE CONSTRUCTED AT 0.6m MINIMUM SPACING FOR ALL PIPES WITH GRADES GREATER THAN 1% IN ACCORDANCE WITH THE SPECIFICATION.
- ALL BOX CULVERT UNITS SHALL BE MANUFACTURED AND CLASSIFIED IN ACCORDANCE WITH AS 1507.
- WHERE EXISTING PIPES ARE TO BE EXTENDED, THE EXISTING PIPE SHALL BE EXPOSED TO EXPOSE THE NEAREST MANUFACTURED JOINT AND THE NEW PIPE JOINED AT THIS POINT USING A MANUFACTURED EXTERNAL BAND.

VERGE MANAGEMENT

- PARKS CONSERVATION AND LANDS PACS) TO BE CONTACTED PRIOR TO THE COMMENCEMENT OF SITE WORKS TO OBTAIN CERTIFICATE OF COMPLIANCE FOR THE VERGE MANAGEMENT PLAN ON COMMENCEMENT AND COMPLETION OF VERGE RESTORATION.
- THERE SHALL BE NO PARKING, SITE OFFICES, BILLBOARDS OR STORAGE OF MATERIALS ON ANY VERGE UNDER ANY CIRCUMSTANCES. PROTECT ALL GRASSLAND TREES AND SHRUBS OUTSIDE THE WORKS AREA FROM DAMAGE.
- ANY WORKS THAT ALTER OR DISTURB GRASSED VERGE AREAS, MEDIANS OR OTHER OPEN AREAS MUST BE REINSTATED TO EXISTING CONDITION BY THE PERSON(S) RESPONSIBLE FOR THE INTERFERENCE IN ACCORDANCE WITH THE GENERAL LANDSCAPE OR CARE PLAN. THE LATTER AGREEMENT AT THE DRYLAND GRASS MAINTENANCE CONTAINS TALL FESCUE IT IS TO BE DIVERSITY TYPICAL FESCUE.
- TEMPORARY FENCING TO PROTECT THE SITE TREES SHALL BE 1.8m HIGH CHAIN MESH FENCE WITH BODIE CLOTH ATTACHED.
- ALL EXISTING TREES TO BE REMOVED SHALL BE REMOVED PRIOR TO THE COMMENCEMENT OF VERGE RESTORATION.
- THE FENCING IS TO REMAIN CONTINUOUS THROUGHOUT THE PROJECT, EXCEPT WHERE SERVICE TRENCHING OCCURS. DISTURBANCE TO BE REINSTATED TO EXISTING CONDITION BY THE PERSON(S) RESPONSIBLE FOR THE INTERFERENCE IN ACCORDANCE WITH THE GENERAL LANDSCAPE OR CARE PLAN. THE LATTER AGREEMENT AT THE DRYLAND GRASS MAINTENANCE CONTAINS TALL FESCUE IT IS TO BE DIVERSITY TYPICAL FESCUE.
- ALL TREES ARE TO BE RETAINED AND LEFT UNDISTURBED UNLESS SHOWN OTHERWISE. EXISTING CROWN CLEARANCE NOT TO BE ALTERED.
- FOR THE PROTECTION OF THE TREE CANOPY AND ROOTS, THE FOLLOWING MEASURES ARE TO BE TAKEN:
 - CROWN AND APICE CANOPY NOT TO BE CUT OR DAMAGED.
 - ENSURE LIFTING EQUIPMENT AND LOAD CAN CLEAR HEIGHT AND WIDTH OF THE CROWN AND BRANCHES.
 - ANY SERVICE CONNECTIONS WITHIN 2M OF AN EXISTING TREE MUST BE APPROVED BY PACS EXCAVATION THAT FOCUSES WITHIN THE DROP ZONE OF A TREE MUST BE APPROVED AND RESTRICTED TO ONE SIDE OF THE TREE. ONLY WHERE APPROVED SHALL BE APPROVED.
 - MEASURES ARE TO BE ADAPTED FOR TREE PROTECTION.
 - DO NOT SEVER LARGE ROOTS (BROWN) CLOSER THAN HALFWAY FROM THE DRUPLE TO THE TRUNK.
 - ALL ROOTS MUST BE CUT CLEANLY WITH EQUIPMENT SPECIFICALLY DESIGNED FOR TREEWORK OTHER PRUNING EQUIPMENT.
 - ROOTS EXPOSED DURING EXCAVATION MUST BE PROTECTED FROM DECOMPOSITION. KEEP LIGHTLY WATERED. COVER EXPOSURE WHICH MUST BE KEPT MOIST.
 - WATER TREES WHICH HAVE HAD DISTURBANCE IN THEIR ROOT ZONE, THE AMOUNT AND FREQUENCY OF WATERING NEEDS TO BE ADAPTED TO THE TREE'S REQUIREMENT, BASED ON SEASONAL CONDITIONS.
- VERGE RESTORATION IS TO BE OVERSEEN BY A LANDSCAPE ARCHITECT.

CONDITIONS OF CONTRACT COMPLIES

This document is to be used in compliance with the conditions as listed in the Statement of Work and/or the contract documents.

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Signed: *[Signature]* Date: 31.10.19

Contact phone: 6285 7705

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Cardno
Shaping the Future

Cardno (NSWACT) Pty Ltd | ABN 95 001 145 035
Level 2, 14 Wormald Street
Sydney NSW ACT 2010
Tel: 02 6112 4500 Fax: 02 6112 4539
Web: www.cardno.com.au

Drawn	14/06/2018	Date	14/06/2018
Checked	14/06/2018	Date	14/06/2018
Designed	14/06/2018	Date	14/06/2018
PO/PL	14/06/2018	Date	14/06/2018
Verified	14/06/2018	Date	14/06/2018
Approved	14/06/2018	Date	14/06/2018

CAPTIAL RECYCLING SOLUTIONS PTY LTD

Project: PART B11 S47 FRESHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY) E9 AND B11 S8 FRESHWICK (ACCESS AND SW CONNECTION)

Scale: 1:1000

Revision: A1

Drawing Number: 50518089-C1-1005

Revision: K

NOT TO BE USED FOR CONSTRUCTION PURPOSES

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From: [Emily Leemhuis](#)
To: [Riches, Dominic](#)
Cc: [DiCampli, Nicki](#)
Subject: RE: CRS DA's (201834656 & 201835108)
Date: Monday, 4 November 2019 9:55:11 AM
Attachments: [image001.png](#)
[image002.jpg](#)
[Demolition checklist.pdf](#)
[50518089-1020-PAVF-1.pdf](#)
[Conditional Acceptance179970.pdf](#)
[50518089-1041-FMCP-1.pdf](#)
[50518089-1005-GA-2.pdf](#)
[50518089-1005-GA-3.pdf](#)
[50518089-1005-GA-4.pdf](#)
[50518089-1005-GA-1.pdf](#)
[50518089-1005-GA-5.pdf](#)

Hi Dominic,

See attached Icon approvals for DA201835108.

Regards,

Emily Leemhuis Senior Urban Planner	Purdon Planning Pty Ltd Unit 4, Cooyong Centre 1 Torrens Street Braddon ACT 2612	T: 02 6257 1511 www.purdon.com.au
------------------------------------------------------	-------------------------------------------------------------------------------------------	-----------------------------------------------------------------------------



From: Riches, Dominic <Dominic.Riches@act.gov.au>
Sent: Tuesday, 29 October 2019 5:00 PM
To: Emily Leemhuis [REDACTED]
Cc: DiCampli, Nicki <Nicki.DiCampli@act.gov.au>
Subject: RE: CRS DA's (201834656 & 201835108)

UNOFFICIAL

Hi Emily

Stage 6 has reviewed the documentation and has discussed with Icon Water the above DAs.

Icon Water have informed us that the Statement of Conditional Acceptance (application No: 179611) only applies to DA201834656. Icon Water have stated that a separate application is required, however, if provided quickly, could be finalised by the end of the week. Stage 6 have also confirmed that once the statement of conditional acceptance is provided then they can act quickly to finalise.

Bibek Ligal (Icon Water) will contact you to resolve.

Regards

Dominic Riches | Post-DA Review
Phone 02 6205 1834
Planning Delivery

From: Riches, Dominic
Sent: Monday, 28 October 2019 3:37 PM
To: Emily Leemhuis [REDACTED]
Subject: RE: CRS DA's (201834656 & 201835108)

UNOFFICIAL

Thanks Emily

I have discussed with the stage 6 team who said they should be able to send it out pretty quickly.

Regards

Dominic Riches | Post-DA Review
Phone 02 6205 1834
Planning Delivery

From: Emily Leemhuis [REDACTED]
Sent: Monday, 28 October 2019 3:26 PM
To: Riches, Dominic <Dominic.Riches@act.gov.au>
Subject: CRS DA's (201834656 & 201835108)

Hi Dominic,

Following on from our discussion the other week re additional information required prior to release of stamped plans, please see attached the following documents for your consideration:

- Updated plan stating that the acoustic fence is in accordance with the Rudds plan
- Icon Water Conditional Acceptance (both DA)
- Amended lighting plan with correct AS references

Let me know if there's anything further outstanding.

Best Regards,

Emily Leemhuis

Senior Urban Planner

Purdon Planning Pty Ltd
Unit 4, Cooyong Centre
1 Torrens Street

T: 02 6257 1511

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Braddon ACT 2612



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STATEMENT OF CONDITIONAL ACCEPTANCE

Application No: 179970 **Suburb:** Fyshwick **Block/Section** 11 / 47

Appcn Type: Non residential/New Construction Inclusions : Car Park/driveway,
Landscaping, Other, Retaining walls/fencing

Attached Plans

50518089-1041-EMCP-1.pdf
50518089-1005-GA-2.pdf
50518089-1005-GA-3.pdf
50518089-1005-GA-4.pdf
50518089-1005-GA-1.pdf
50518089-1005-GA-5.pdf
50518089-1020-PAVE-1.pdf

Conditions of Acceptance

A water network asset is located on this block. Unobstructed 24 hour - 7 day a week access is to be maintained across the land to the asset. As per Criterion 1 - Easement and other access Clearances of the Planning and Development Regulation 2008. No structures are to be built over Icon Water easements, pipe protection envelopes or access passages without approval from Icon Water in writing.

All Icon Water Valves, Hydrants, Meters and Maintenance Holes/Pits must have 24/7 access by Icon Water and Emergency Services. The developer must provide Icon Water 24/7 access to the construction site. The developer is to obtain Icon Water padlocks which must be installed in a daisy chain fashion at each entry and exit point of the construction site. No stockpiles, Temporary structures or equipment is to be located within the pipe protection envelope.

Changes to the lease for use of the land and further development of the block may require development or augmentation to the water and/or sewer networks. Augmentation of the networks associated with further development of the block are to be funded by the developer. A charge under the Water and Sewerage Capital Contributions Code may apply. If multiple blocks are created, separate ties to sewer and water will also be required for each block. Separate access will be required through each block,

Icon Water accepts only standard concrete with expansion joints, and maximum 100mm density within sewer easement or pipe protection envelope.

Icon Water assets shall be protected for the duration of the construction works from short term load shedding from construction machinery or vibration and groundwater ingress or infiltration. Any damage to Icon assets resulting from the construction works shall be repaired by Icon Water at the contractors expense. Access to Icon's assets including sewer structures, manholes, hydrants and valves is to be maintained for the duration of the construction works.

Other

Protection of water and sewer networks to be in accordance with the attached checklist

Working on or near Icon Water Assets- the detail design will include the instruction to the contractors to submit 'SWMS' for Icon Water for approval prior to commencement of work. Example of works: excavation; backfilling; compaction; heavy equipment usage & access over existing mains, etc. Detailed design must be approved by Icon Water prior to commencement of works.

This conditional acceptance is in relation to DA 201835108 and DA 201834656 however strictly subjected to works as per Icon Water building approval 179611 being completed prior to taking effect. The developer is to ensure that any variations to the relocation of Water mains works as per approval 179611 has to be assessed and approved by Icon Wat

Please Note:

- ❖ Separate applications are required for ActewAGL electricity and gas networks.
- ❖ Any failure to accurately show existing or proposed structures may result in damage and costs for which the property owner will be liable. Damage to network assets must be reported to Icon Water.
- ❖ Utility conditions may also apply to minor structures and landscaping works that may not have been depicted in this document. Consultation with Icon Water is advised if such structures and landscaping are planned.
- ❖ Icon Water recommends setbacks provided for equipment access to water or sewerage network assets are kept free of obstruction by the property owner.
- ❖ Construction of works without the approval of Icon Water may result in connection to Icon Water's water and sewerage networks being denied and /or action being taken to require the property owner to remove non-compliant services (e.g. sanitary drains) or structures.

WARNING

Failure to comply with these conditions is likely to result in interference with an Icon Water sewer or water asset. Part 5 of the Utilities (Technical Regulation) Act 2014 applies, which indicates a person who interferes with a utility asset may be subject to prosecution. The current maximum penalty is 200 penalty units, imprisonment for two years, or both. In addition the land-holder can be ordered at their expense to stop the interference, which may involve removal of the building work or that part of the building work that is causing, or is likely to cause interference.

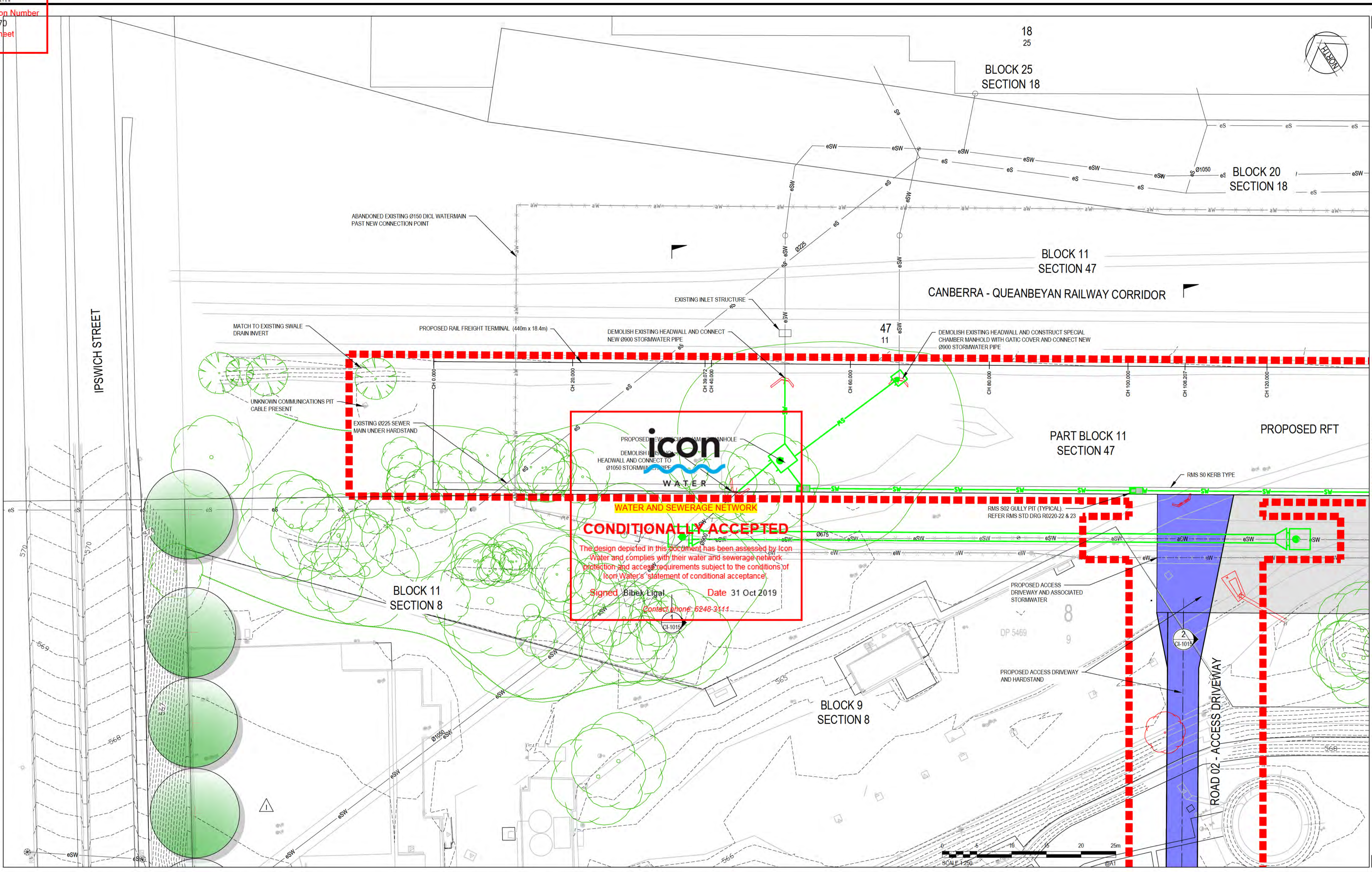
Comments:

Signed Bibek Ligal **Date** 31 Oct 2019

For further information please phone Icon Water 6248 3111.

icon
 Application Number
 970
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 9

XREFs: x_Existing services; x_CD Background; x_Survey; x_Survey Conts; x_TREES; x_Base-2
 CAD File: N:\Projects\50518089_CRS RAIL SIDING B11 S47 FYSHWICK\Drawings\Build\Block 11_Section 47\50518089-1005-GA.dwg
 DATE PLOTTED: 30 October 2019 4:00 PM EST
 JOINS 50518089-CI-1007



icon
 WATER
WATER AND SEWERAGE NETWORK
CONDITIONALLY ACCEPTED
 The design depicted in this document has been assessed by Icon Water and complies with their water and sewerage network protection and access requirements subject to the conditions of Icon Water's 'statement of conditional acceptance'.
 Signed: Bibek Ligal Date: 31 Oct 2019
 Contact phone: 6248 3111



Rev	Date	Description	Des	Vent	Appd
I	28/06/2019	RE-SUBMISSIONS	GH	AS	GZ
H	14/03/2019	RE-SUBMISSIONS	PDJ	TL	GZ
G	19/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
F	18/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
E	11/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
D	24/07/2018	DA SUBMISSIONS - COMMENTS	PDJ	TL	GZ
C	14/06/2018	REVISED DA SUBMISSION	PDJ	TL	GZ
B	23/05/2018	DA SUBMISSION	PDJ	TL	GZ
A	11/05/2018	CLIENT REVIEW	PDJ	TL	GZ



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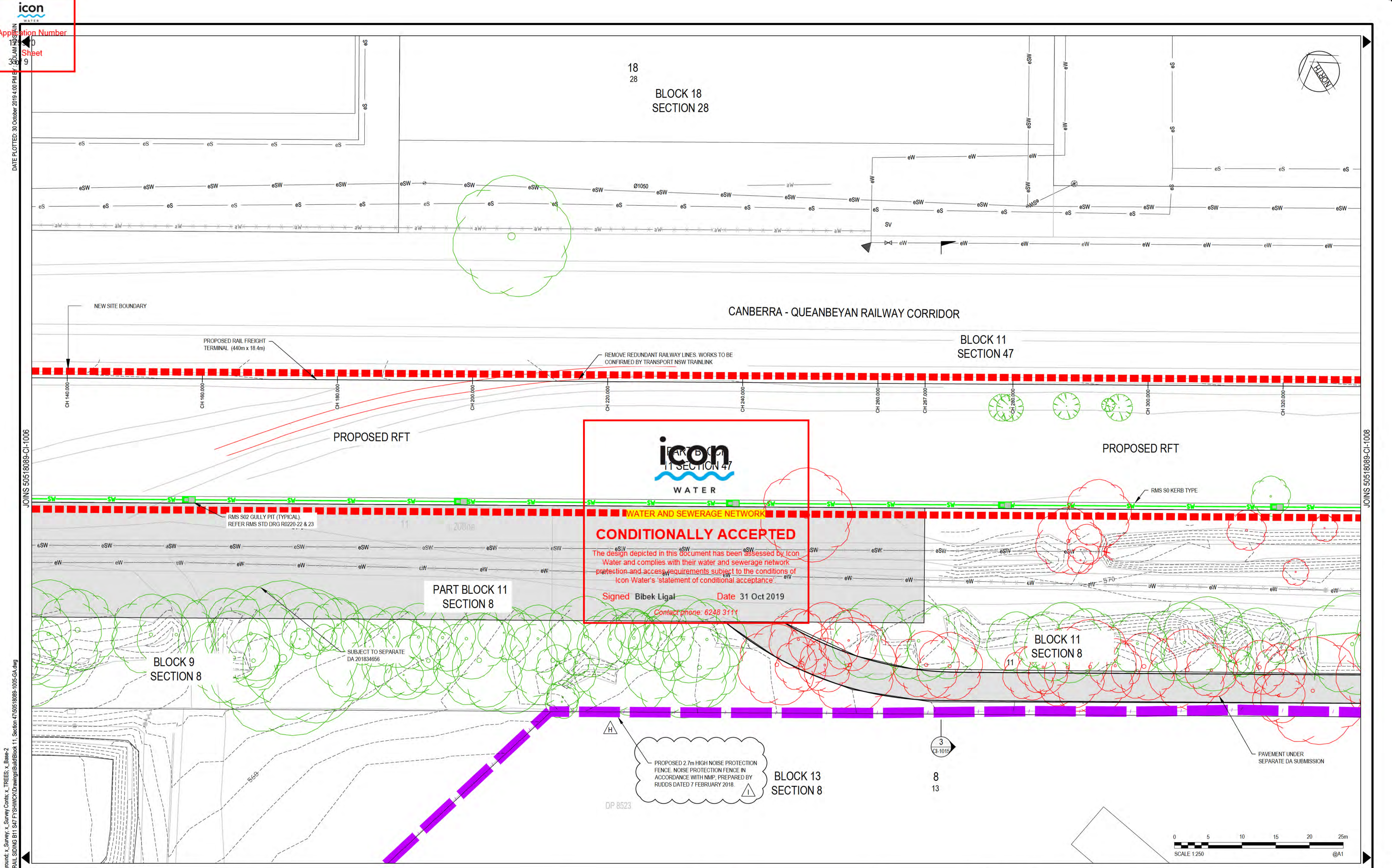
Drawn	Date	Client
PDJ	14/06/2018	CAPTIAL RECYCLING SOLUTIONS PTY LTD
Checked	Date	Project
TL	14/06/2018	PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY)
Designed	Date	Status
PDJ/TL	14/06/2018	FOR APPROVAL
Verified	Date	NOT TO BE USED FOR CONSTRUCTION PURPOSES
GZ	14/06/2018	
Approved	Date	Datum
JPS	14/06/2018	AHD
		Scale
		1:250
		Size
		A1
		Drawing Number
		50518089-CI-1006
		Revision
		I

Client: CAPTIAL RECYCLING SOLUTIONS PTY LTD
 Project: PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY)
 B9 AND B11 S8 FYSHWICK (ACCESS AND SW CONNECTION)
 Title: GENERAL ARRANGEMENT PLAN SHEET 1 OF 3

Status		FOR APPROVAL	
NOT TO BE USED FOR CONSTRUCTION PURPOSES			
Datum	Scale	Size	
AHD	1:250	A1	
Drawing Number	Revision		
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Application Number
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Rev	Date	Description	Des	Vent	Appd
I	24/10/2019	NOTES UPDATED	GH	AS	GZ
H	28/06/2019	RE-SUBMISSIONS	GH	AS	GZ
G	19/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
F	18/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
E	11/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
D	24/07/2018	DA SUBMISSIONS - COMMENTS	PDJ	TL	GZ
C	14/06/2018	REVISED DA SUBMISSION	PDJ	TL	GZ
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A	11/05/2018	CLIENT REVIEW	PDJ	TL	GZ



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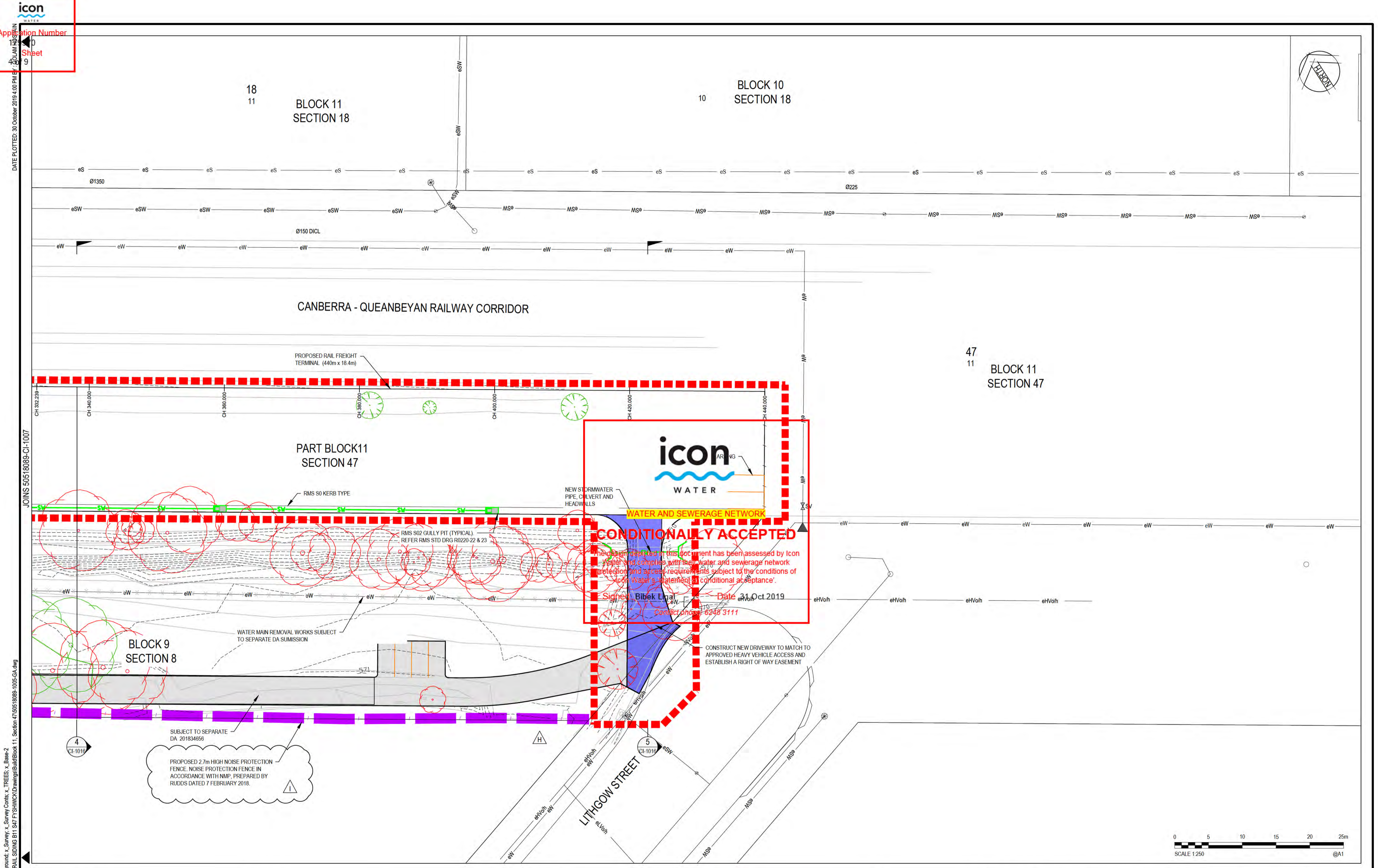
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TL	14/06/2018
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PDJ/TL	14/06/2018
Verified	Date
GZ	14/06/2018
Approved	Date
JPS	14/06/2018

Client	
CAPITAL RECYCLING SOLUTIONS PTY LTD	
Project	
PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY) B9 AND B11 S8 FYSHWICK (ACCESS AND SW CONNECTION)	
Title	
GENERAL ARRANGEMENT PLAN SHEET 2 OF 3	

Status		
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NOT TO BE USED FOR CONSTRUCTION PURPOSES		
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Drawing Number	Revision	
50518089-CI-1007	I	

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DATE PLOTTED: 30 October 2019 4:00 PM E:\Projects\50518089\CAD File: N:\Projects\50518089_CRS RAIL SIDING B11 S47 FYSHWICK\Drawings\Build\Block 11, Section 47\50518089-1005-GA.dwg

Rev	Date	Description	Des	Vent	Appd
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D	24/07/2018	DA SUBMISSIONS - COMMENTS	PDJ	TL	GZ
C	14/06/2018	REVISED DA SUBMISSION	PDJ	TL	GZ
B	23/05/2018	DA SUBMISSION	PDJ	TL	GZ
A	11/05/2018	CLIENT REVIEW	PDJ	TL	GZ

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Recycling Solutions

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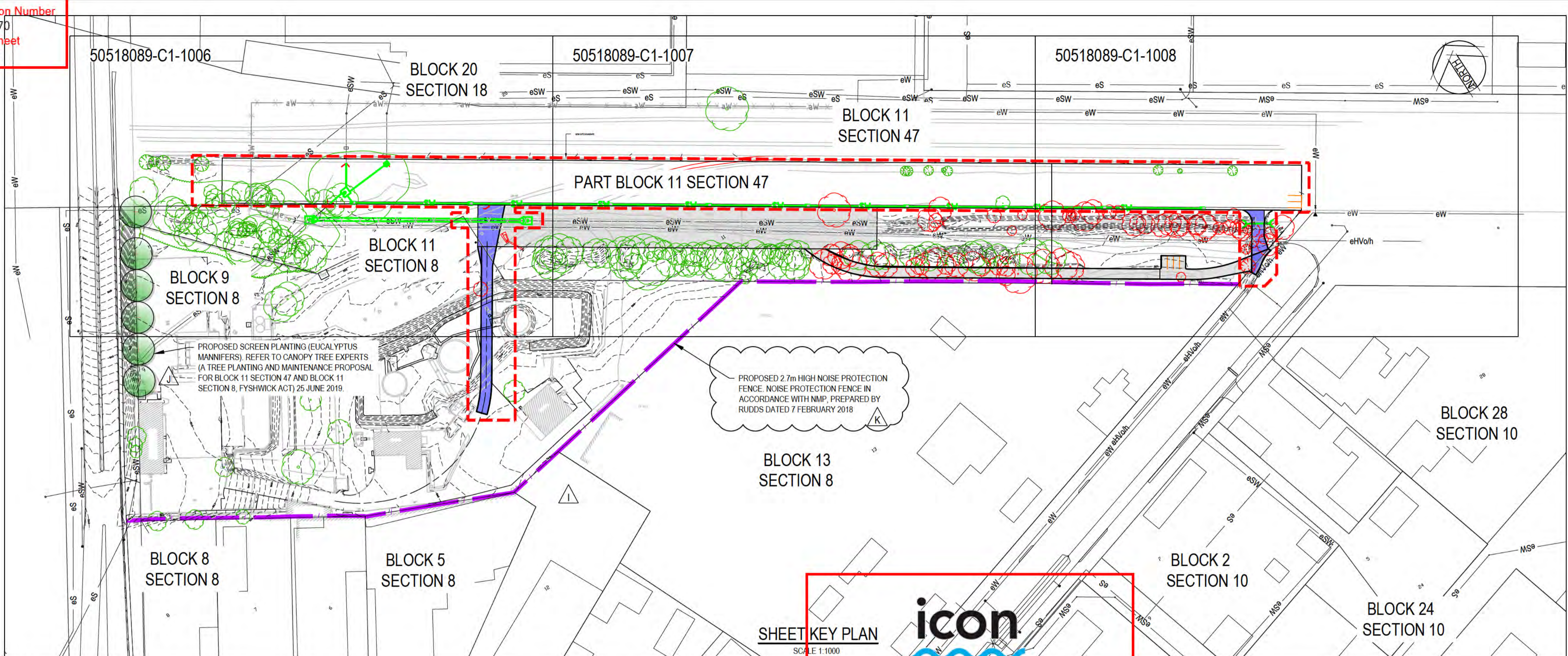
Drawn	Date	Client
PDJ	14/06/2018	CAPTIAL RECYCLING SOLUTIONS PTY LTD
Checked	Date	Project
TL	14/06/2018	PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY)
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PDJ/TL	14/06/2018	FOR APPROVAL
Verified	Date	Datum
GZ	14/06/2018	AHD
Approved	Date	Scale
JPS	14/06/2018	1:250

Title	Revision
GENERAL ARRANGEMENT PLAN	
SHEET 3 OF 3	

Drawing Number	Revision
50518089-CI-1008	I



Application Number
970
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LEGEND

- STAGE BOUNDARY
- PROPOSED 2.7m HIGH NOISE PROTECTION FENCE
- PROPOSED RAIL FREIGHT TERMINAL
- PROPOSED ACCESS DRIVEWAYS (BLOCK 9 & 11, SECTION 8)
- PAVEMENTS SUBJECT TO SEPARATE DA SUBMISSION (201834665)
- PROPOSED SCREEN PLANTING (EUCALYPTUS MANNIFERS)
- EXISTING TREE TO BE RETAINED
- EXISTING TREE TO BE REMOVED

PROPOSED SERVICES

- STORMWATER
- SWALE / CUT OFF DRAIN
- SAWCUT AND/OR MATCH TO EXISTING SMOOTHLY

EXISTING SERVICES

- STORMWATER/MANHOLE
- WATER
- SEWER/MANHOLE
- COMMUNICATIONS - TELSTRA
- COMMUNICATIONS - NEXTGEN
- GAS
- ELECTRICAL - OVERHEAD HIGH VOLTAGE
- ELECTRICAL - STREET LIGHT
- EXISTING STREET LIGHT
- SERVICE TO BE REMOVED

- ### GENERAL NOTES
- ALL CONSTRUCTION WORK SHALL BE CARRIED OUT IN ACCORDANCE WITH THE SPECIFICATION AND WHERE WORKS OCCURS ON TERRITORY LAND IN ACCORDANCE WITH THE ACT STANDARD SPECIFICATION FOR URBAN INFRASTRUCTURE.
 - ALL CONSTRUCTION WORK SHALL BE CO-ORDINATED WITH ADJACENT CONTRACTS.
 - THE LOCATION OF EXISTING SERVICES ARE SHOWN IN THEIR APPROXIMATE LOCATION ONLY. PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ON SITE, THE CONTRACTOR SHALL CONTACT THE RELEVANT AUTHORITIES AND/OR ASSET OWNERS/MAINTAINERS AND VERIFY THE LOCATION OF ALL UNDERGROUND SERVICES AND OBTAIN NECESSARY CLEARANCES.
 - THE CONTRACTOR SHALL ENSURE THAT THE INSTALLATION OF UNDERGROUND SERVICES ARE CO-ORDINATED AND PLANNED PRIOR TO COMMENCING INSTALLATION. NO DEVIATIONS FROM NOMINATED ALIGNMENT/ZONES IS PERMITTED.
 - THE CONTRACTOR SHALL GIVE ACT ROADS SUFFICIENT NOTIFICATION TO ALLOW EXISTING ROAD PAVEMENT MAINTENANCE TO OCCUR WHERE THE EXISTING PAVEMENT HAS FAILED AND WILL NOT ALLOW THE SATISFACTORY MATCHING OF THE NEW PAVEMENT.
 - SURFACES WHICH LIE OUTSIDE THE GENERAL LIMITS OF LANDSCAPING AND RESTORATION WHICH ARE DISTURBED DURING THE CONSTRUCTION OF THE WORKS SHALL BE RESTORED BY THE CONTRACTOR, AT THE CONTRACTOR'S EXPENSE, TO AT LEAST THEIR PRE-CONSTRUCTION CONDITION. THESE SURFACES INCLUDE BUT ARE NOT NECESSARILY LIMITED TO, PAVEMENTS, PAVING, GRASSING, ETC.
 - ALL DESIGN SUBGRADE CBR VALUES FOR PAVEMENTS AND PARKING MUST BE CONFIRMED BY THE CONTRACTOR PRIOR TO COMMENCEMENT OF CONSTRUCTION. RESULTS OF ADDITIONAL SUBGRADE TESTS MUST BE MADE AVAILABLE TO THE SUPERINTENDENT WITHIN 24 HOURS OF RECEIPT. IF THE CBR IS LESS THAN THE CURRENT DESIGN THEN A PAVEMENT RE-DESIGN WILL BE REQUIRED BEFORE PROCEEDING.
 - ALL SERVICE PIT COVERS LOCATED IN FOOTPATHS AND ROADWAYS SHALL BE CLASS D GATIO COVERS UNO AND BE INFILLED WITH THE SAME MATERIAL AS SURROUNDING MATERIAL.
 - ALL SERVICE PIT COVERS LOCATED IN FOOTPATHS AND SHARED ZONES OR VERGES SHALL BE ORIENTATED PARALLEL OR PERPENDICULAR TO THE PRECINCT BOUNDARY.
 - ALL GRATED SUMPS SHALL BE FITTED WITH HEEL GUARD COVERS.
 - ALL NEW SERVICES / UTILITIES INFRASTRUCTURE TO BE INSTALLED WITH NECESSARY UNDERGROUND MARKING TAPE IN ACCORDANCE WITH AS 2648.

- ### STORMWATER PIPE NOTES
- Ø100 AND Ø150 : uPVC CLASS SN10 AND SN8, RESPECTIVELY TO AS1260 PARTS 1 TO 4 WITH SOLVENT WELDED JOINTS EXCEPT WHERE RUBBER RING JOINTS ARE REQUIRED ADJACENT TO SUMPS TO ALLOW MOVEMENT. UNO.
 - ALL uPVC PIPES SHALL BE INSTALLED IN ACCORDANCE WITH AS2032 AND THE SPECIFICATION.
 - INTERNAL STORMWATER DRAINAGE SERVICE TIES SHALL HAVE A MINIMUM GRADE OF 1% AND BE END CAPPED.
 - INSTANT STRIP PACKERHEAD AND VIBRATED VERTICALLY CAST CONCRETE PIPES AND SIMILARLY MANUFACTURED CONCRETE PIPES SHALL NOT BE USED.
 - ALL JUNCTIONS SHALL BE MADE WITH MANUFACTURED FITTINGS.
 - ALL BACKFILLING UNDER ROAD AND CARPARK PAVEMENTS AND PAVING SHALL BE IN ACCORDANCE WITH THE SPECIFICATION AND THE DRAWINGS.
 - CONNECTION TO STORMWATER PIPES WITH A BRANCH CONNECTION OF Ø100 OR Ø150 uPVC SHALL BE WITH FRC SADDLE ADAPTOR OR A uPVC BRANCH. REFER TOCS STD DRG No ST-0001
 - MINIMUM GRADES ON Ø100 AND Ø150 uPVC STORMWATER SHALL BE 1.0% UNLESS NOTED OTHERWISE.
 - ALL PIPE LENGTHS, PIPE INVERT LEVELS AND GRADES ARE MEASURED AT CENTRE OF STORMWATER STRUCTURE.
 - STEP IRONS ARE REQUIRED IN SW SUMPS AND MANHOLES THAT ARE DEEPER THAN 900mm REFER TOCS DESIGN STANDARDS.
 - SCOUR STOPS FOR STORMWATER PIPES SHALL BE CONSTRUCTED AT 5.0m MAXIMUM SPACING FOR ALL PIPES WITH GRADES GREATER THAN 7% IN ACCORDANCE WITH THE SPECIFICATION.
 - ALL BOX CULVERT UNITS SHALL BE MANUFACTURED AND CLASSED IN ACCORDANCE WITH AS 1597.
 - WHERE EXISTING PIPES ARE TO BE EXTENDED, THE EXISTING PIPE SHALL BE EXHUMED TO EXPOSE THE NEAREST MANUFACTURED JOINT AND THE NEW PIPE JOINED AT THIS POINT USING A MANUFACTURED EXTERNAL BAND.

WATER VERGE MANAGEMENT

WATER AND SEWERAGE NETWORK

CONDITIONALLY ACCEPTED

The design depicted in this document has been assessed by icon Water and complies with their water and sewerage network protection and access requirements subject to the conditions of icon Water's 'statement of conditional acceptance'.

Signed Bibek Ligal
Date 31 Oct 2019
Contact phone: 6248 5171

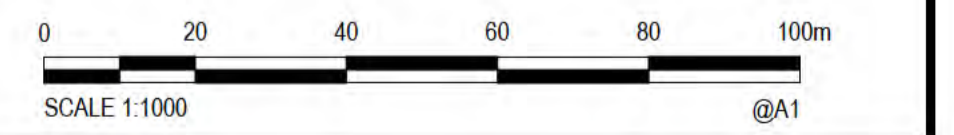
- ### STORMWATER STRUCTURE NOTATION
- PS1 DENOTES PLANTATION SUMP REFER STD DRG No ST-0013 FOR DETAILS
 - GS GRATED SUMP
 - SCMH SPECIAL CHAMBERED MANHOLD
 - Ø130 DENOTES STORMWATER STRUCTURE No. DENOTES STORMWATER LINE No.
- ALL STRUCTURE COVERS SHALL BE DESIGNED AND INSTALLED IN ACCORDANCE WITH AS3966

- ### SUBSOIL DRAINAGE NOTES
- ALL SUBSOIL DRAINS SHALL BE INSTALLED IN ACCORDANCE WITH THE SPECIFICATION
 - ALL SUBSOIL DRAINAGE SHALL BE Ø100 CLASS SN10 IN ACCORDANCE WITH AS 2439.1.
 - Ø100 uPVC CLASS SN8 SHALL BE USED WHERE SUBSOIL PIPE CROSSES ROAD PAVEMENTS, MIN GRADE 1.0%
 - CONNECTION OF SUBSOIL DRAINS TO SUBSOIL DRAINS SHALL BE WITH MANUFACTURED FITTINGS AND DUCT TAPED TO SECURE PIPE TO FITTING, INCLUDING JOINING ONTO EXISTING SSD.
 - GRADE SUBSOIL AT MINIMUM 1% FROM OUTLET POINT.
 - COMPACTION OF 7mm CRUSHED ROCK FILTER MATERIAL TO BE:
 - A) IN LANDSCAPED AREAS - COMPACTED TO DENSITY INDEX 70%
 - B) IN PAVED AREAS:
 - i) IN TOP 300mm BELOW SUBGRADE - COMPACTED TO DENSITY INDEX 90%
 - ii) IN LOWER LAYERS - COMPACTED TO DENSITY INDEX 80%

- ### REINSTATEMENT OF TRENCHES
- ALL TRENCHES SHALL BE BACKFILLED AND COMPACTED IN ACCORDANCE WITH THE DRAWINGS AND SPECIFICATION. SURFACE FINISHES SHALL BE REINSTATED TO MATCH WITH EXISTING ABUTTING SURFACES WITHOUT STEPPING AND LIPPING.

- ### LANDSCAPING
- ALLOW IN THE WORKS THE NECESSARY SET-DOWNS, LEVELS AND GRADES FOR LANDSCAPE FINISHES TO BE APPLIED. REFER TO LANDSCAPE ARCHITECT'S DRAWINGS FOR DETAILS OF FINISHES.

- ### REINFORCEMENT
- ALL REINFORCEMENT SHALL HAVE 500MPa TENSILE STRENGTH TO THE SIZES/TYPE SHOWN ON THE DRAWINGS.



XREFs: x_Existing services; x_CD Background; x_Survey; x_Survey; x_Trees; x_Base-2
CAD File: N:\Projects\05\FY18\089_CRS_RAIL_SIDING B11 S47 FYSHWICK\Drawings\Build\Block 11_Section 47\0518089-CI-1005-GA.dwg

Rev	Date	Description	Des	Vent	Appd
K	24/10/2019	NOTES UPDATED	GH	AS	GZ
J	28/06/2019	RE-SUBMISSIONS	GH	AS	GZ
I	28/06/2019	RE-SUBMISSIONS	GH	AS	GZ
H	14/03/2019	RE-SUBMISSIONS	PDJ	TL	GZ
G	19/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
F	18/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
E	11/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
D	24/07/2018	DA SUBMISSIONS - COMMENTS	PDJ	TL	GZ
C	14/06/2018	REVISED DA SUBMISSION	PDJ	TL	GZ



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Cardno
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Drawn	Date	Client
PDJ	14/06/2018	CAPTIAL RECYCLING SOLUTIONS PTY LTD
Checked	Date	
TL	14/06/2018	
Designed	Date	
PDJ/TL	14/06/2018	
Verified	Date	
GZ	14/06/2018	
Approved	Date	
JPS	14/06/2018	

Project PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY) B9 AND B11 S8 FYSHWICK (ACCESS AND SW CONNECTION)

Status **FOR APPROVAL**
NOT TO BE USED FOR CONSTRUCTION PURPOSES

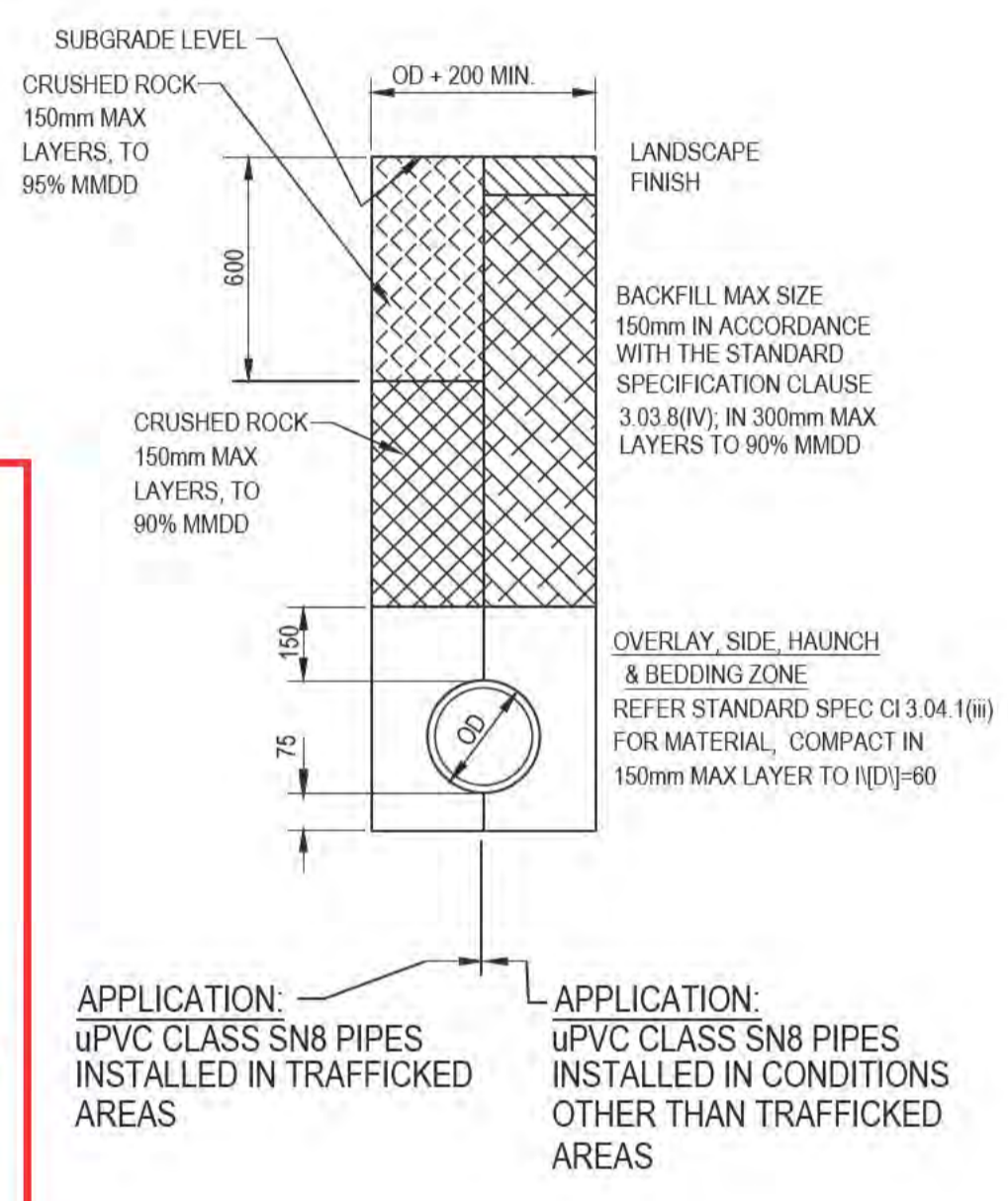
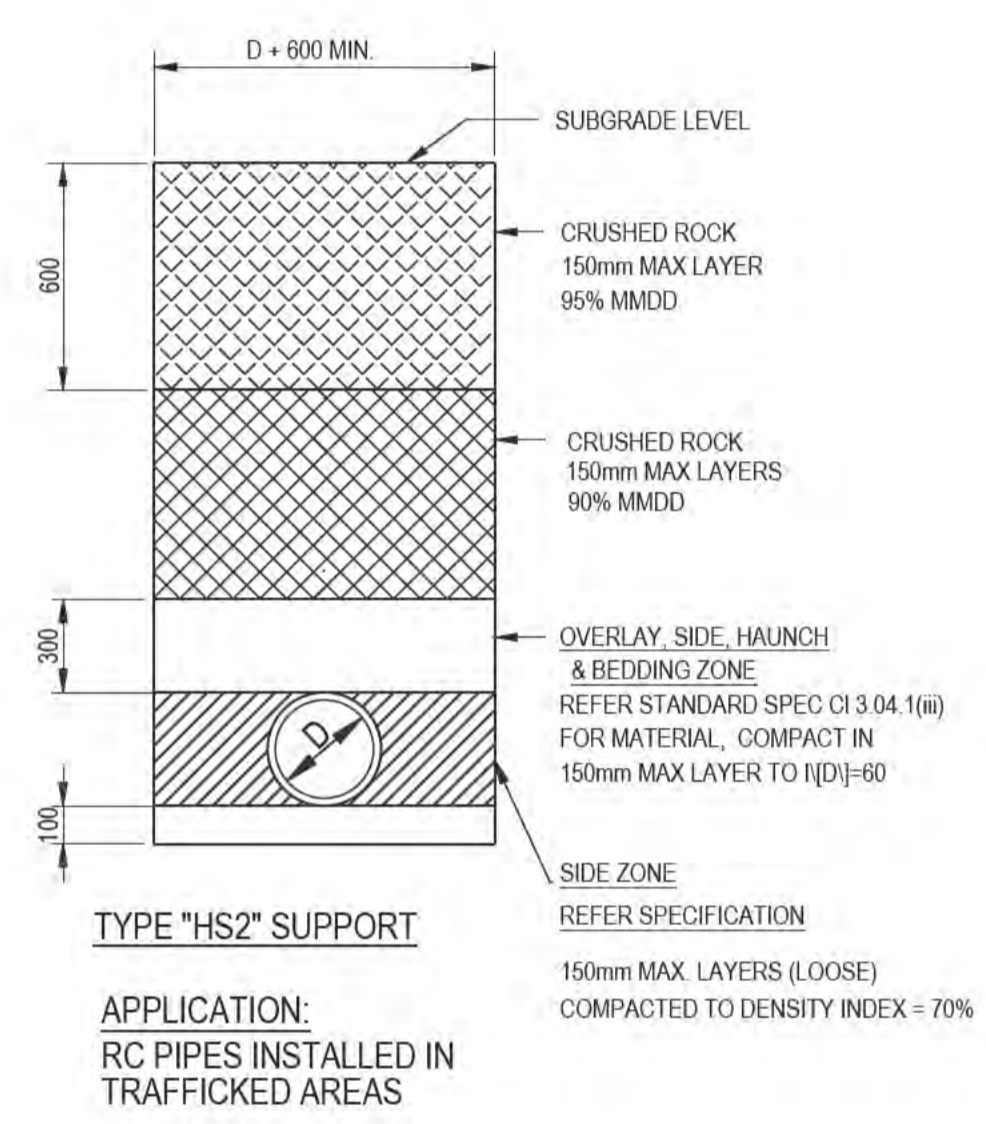
Title GENERAL ARRANGEMENT PLAN
NOTES, LEGEND AND SHEET KEY

Datum AHD Scale 1:1000 Size A1

Drawing Number 50518089-CI-1005 Revision K

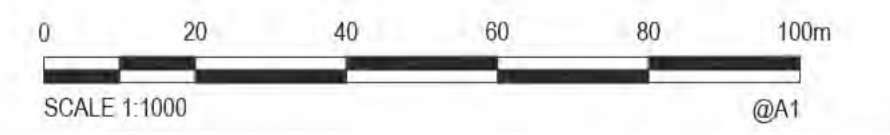
Application Number
15970
Sheet
08 of 9

DATE PLOTTED: 30 October 2019 4:00 PM



STORMWATER PIPES BACKFILL CONDITIONS
NTS

- NOTES**
- FOR DETAILED MATERIAL SPECIFICATIONS AND COMPACTION REQUIREMENTS REFER BASIC SPECIFICATION AND AS 3725 - 2007
 - "D" IS NOMINAL PIPE DIAMETER
 - CRUSHED ROCK IN ACCORDANCE WITH CLAUSE 3.03.8(i) OF STANDARD SPECIFICATION



XREFs: x_TCD Background; x_Survey; x_Survey; x_Survey; x_Trees; x_Base-2; CAD File: N:\Projects\505\FY18089_CRS RAIL SIDING B11 S47 FYSHWICK\Drawings\Build\Block 11_Section 47\50518089-1005-GA.dwg

Rev	Date	Description	Des	Vent	Appd
F	18/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
E	11/12/2018	RE-SUBMISSIONS	PDJ	TL	GZ
D	24/07/2018	DA SUBMISSIONS - COMMENTS	PDJ	TL	GZ
C	14/06/2018	REVISED DA SUBMISSION	PDJ	TL	GZ
B	23/05/2018	DA SUBMISSION	PDJ	TL	GZ
A	11/05/2018	CLIENT REVIEW	PDJ	TL	GZ



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Drawn	PDJ	Date	14/08/2018
Checked	TL	Date	14/08/2018
Designed	PDJ/TL	Date	14/08/2018
Verified	GZ	Date	14/08/2018
Approved	JPS	Date	14/08/2018

Client: **CAPTIAL RECYCLING SOLUTIONS PTY LTD**

Project: **PART B11 S47 FYSHWICK (CONSTRUCTION OF A RAIL FREIGHT TERMINAL AND RAIL TRANSPORT FACILITY) B9 AND B11 S8 FYSHWICK (ACCESS AND SW CONNECTION)**

Title: **GENERAL ARRANGEMENT PLAN MISCELLANEOUS DETAILS**

Status: FOR APPROVAL	
NOT TO BE USED FOR CONSTRUCTION PURPOSES	
Datum: AHD	Scale: AS SHOWN
Drawing Number: 50518089-CI-1009	Size: A1
Revision: F	

Demolition Checklist

Sewer connection

Icon Water's requirements for demolition work which could potentially damage Icon Water's assets or result in discharge of stormwater, or other unapproved wastes (other than domestic sewage) to the Icon Water sewerage network.

The property owner (or their design or construction agent) is responsible for the following:

1. Where an Icon Water sewer main traverses a property or an Icon Water sewer main is outside the property boundary (and proposed demolition works may impact on the sewer main's pipe-protection-envelope), accurate sewer network survey plans must be obtained from Icon Water or from a registered surveyor. Sewer asset locations need to be shown on relevant demolition plans including manhole, boundary riser and tie location with off-set dimensions from site boundaries.
2. Recording the condition of manhole covers. Icon Water can be contacted on 6248 3111 to repair damaged components. A failure to identify any faults before demolition commences may result in the property owner (or their contractors) being held responsible to pay for the repair caused by a failure to adequately protect Icon Water's assets during the demolition or building construction works.
3. Engaging a licensed drainer to disconnect the internal sanitary drains before any demolition works commence. The discharge of debris, groundwater, stormwater or other unapproved liquid wastes (other than domestic sewage into the sewerage networks) is an offence under the *Utilities Act 2000*.
4. Identifying, flagging, barricading and protecting the sanitary drainage 'riser' (if installed) and manholes from demolition operations.
5. Where further construction works is planned, sanitary drains are to be temporarily sealed by capping no closer than three metres from the Icon Water tie. Excavation and capping is to be undertaken by licensed drainers at the customer's expense. The capping point is to be staked behind the cap and identified at ground level. Please note the subsequent building contractor will be required to make a new sanitary drainage connection at the designated tie (not at the temporary cap). This may require the removal of old jump-ups in the customer's sanitary drains. When the tie is exposed it is recommended to ask Icon Water to inspect the branch-line to ensure it is in good condition.
6. Where further construction work is not proposed to commence within 12 months from the date demolition commences, permanent disconnection may be required. Please consult Icon Water at this time for a definitive answer.

Demolition Checklist

Water supply

Icon Water's requirements for demolition works which could potentially damage Icon Water assets or result in contamination of the potable water supply.

The property owner (or their design or construction agent) is responsible for the following:

1. Where an Icon Water main traverses a property (and where an Icon Water main is outside the property boundary but proposed demolition works may impact on the water main's pipe-protection-envelope), accurate water network survey plans must be obtained from Icon Water or from a registered surveyor. Water asset locations need to be shown (including mains, valves and hydrants) on relevant demolition plans (with off-set dimensions to site boundaries).
2. Identifying the location of the Icon Water isolation valve and water meter on the site plan (with off-set dimensions from site boundaries).
3. Recording the condition of the water connection pipe, isolation valve, water meter, verge hydrants and verge network valves. Please contact Icon Water on 6248 3111 to repair damaged components. Failure to identify any faults before demolition works commence may result in the property owner (or their contractors) being held responsible to pay for the repair caused by a failure to adequately protect those assets during the demolition or building construction.
4. Engaging a licensed plumber to disconnect the internal plumbing service at the water meter before any demolition commences.
5. Where further construction works are planned, install a temporary hose cock adjacent to the meter. Retain the meter, meter box and hose cock for the duration of any building works. Identify, flag, barricade and protect the meter assembly from demolition operations and accidental damage. To avoid accidental wastage, turn off the isolation valve until required.
6. Where further construction works is not proposed to commence within 12 months from the date demolition commences, permanent disconnection and removal of the meter may be required. Please consult Icon Water at this time for a definitive answer.
7. Flagging and protecting network isolation valves and hydrants (on the verge) from demolition operations and accidental damage.
8. Advising Icon Water when demolition is complete. Icon Water may take the opportunity to install a new isolation valve and meter assembly to the current Icon Water standard.



Brief to Major Project Review Group

Block/s:	11	Agenda Item:	2
Section:	47	Date:	29/05/2019
Suburb:	Fyshwick	DA Number:	201835108
Zone:	TSZ2 – Services Zone & IZ1 Industrial Mixed Use Zone	Address:	Ipswich Street
Representations:	59	Assessment officer:	James Bennett / Dominic Riches
Cleared by:	Impact Manager		
Proposal :	Construction of a rail freight terminal (11/47 Fyshwick) and the construction of a freight transport facility (11/47 Fyshwick and 9 & 11/8 Fyshwick) and driveways and associated works.		
Additional members required:	Click here to enter text.		
Reason for seeking advice:	Proposal received 30 or more representations		

A copy of the development application for this proposal and assessment documents can be found in the Objective 201835108.

KEY ISSUES FOR DISCUSSION

Entities comments:

Icon Water – Not supported:

- Design Acceptance for External Services or off site works must be in principle design approved by Icon Water Hydraulic Assets. Phone Icon Water Asset Acceptance on ph.: 02 6248 3111 or email to hydraulicassetacceptance@iconwater.com.au. This needs to be referred back to Icon Water Building Approvals area for approval prior to any DA/BA Approval by ACTPLA or certifiers.
- Resubmission to Icon Water for DA approval is required after design acceptance in principle is achieved.
- A condition has been incorporated into the notice of decision (refer condition 1).

Environment Protection Authority (EPA):

- EPA did not support the application as recommended noise mitigation measures, from the noise management plan, were not incorporated into the design.
- EPA have clarified that noise protection measures can be dealt with as a condition of approval.
- A condition of approval has been incorporated into the notice of decision (refer condition 2).

ENTITY ADVICE RELEVANT TO DISCUSSION

It should also be noted that Transport Canberra and City Services (TCCS) did not support the proposal relating to flooding and traffic. Further information has been submitted and TCCS now support the proposal.

SUMMARY OF REPRESENTATIONS (list key concerns only)

- Rail Freight Terminal (rail use) is prohibited in IZ2.
- Waste facility and waste incinerator.
- EIS process should be concluded prior to any other DA being determined.
- Other options must be considered through a strategic planning process for the rail freight terminal.
- Viability of the freight transport facility.
- Objectives and suitability of the land.
- Public land management plan.
- An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.
- Documentation is insufficient and lessee details are incorrect.
- Insufficient consultation has been undertaken.
- All entity advice should be considered.
- Noise impacts on surrounding development need to be considered, including noise generated from the railway.
- Traffic impacts on the surrounding area, including cumulative and future impacts.
- Safety and structural integrity around rail line during construction.
- Relocation of utilities and all works should be at the proponent's expense (no government funding for private development).
- Contamination has not been appropriately addressed.
- A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land).

OPTIONS

The planning and land authority is responsible for determining the application and has the following options:

- Approve the application
- Approve the application with conditions
- Remit the application back to the applicant
- Refuse the application
- Other.

RECOMMENDATION

It is recommended that the MPRG support the proposal with conditions.

MPRG MINUTES

Minutes taken by:	Thara Boraiah	Chair:	Jonathan Teasdale
Conflicts of Interest:	No		
Members present:	Jonathan Teasdale, Bob Taylor, Chris Gell, Rumana Jamaly, James Bennett, Dominic Riches and Thara Boraiah; and Gabriel Joseph (TCCS) Rachel Dawes (TPU) Poppy McRae (DDG)		
Additional attendees			
MPRG Advice:	<p>The case officer presented the development proposal to the members including background on other previous and current applications.</p> <p>MPRG decided to support the application subject to conditions</p> <ul style="list-style-type: none"> -as required by the Territory Plan assessment and entity comments' outcome -sub-licence required for 11/47 Fyshwick (current licence provides for yielding up and ongoing rights and liabilities) -condition licence or direct sale for works on 11/8 Fyshwick: TCCS raised matter of existing services on the land and how they will be managed through the direct sale process -if a lease is issued for 11/8 Fyshwick the lease/DP would include utility easements -consolidation of 9 & 11/8 Fyshwick (required by direct sale) -Leasing comments require a transfer and grant of easement -Trees-no conditions required 		
Matters to be noted or actioned from MPRG discussion:	<p>Conditions to be applied as per above</p> <p>Utility and asset issues to be resolved through licence/direct sale process</p>		
Date minutes cleared:	Click here to enter a date.		
Cleared by:	Choose an item.		
Is the decision to undergo quality assurance?	Click here to enter text.		



DA ASSESSMENT – Merit Track

Block/s:	9	DA number:	201835108
Section:	8	Date lodged:	30-Jan-19
Suburb:	Fyshwick	Due date:	04/04/2019
Zone/s:	IZ2/TSZ2	Unit Number (if applicable)	
Proposal:	Construction and use of Block 11 Section 47 Fyshwick for a rail freight terminal, and for the construction and use of Block 11 Section 47, Block 11 Section 8, and Block 9 Section 8 Fyshwick for a freight transport facility, and driveways and associated works.		
Proposed Use:	IZ2 – Industrial Mixed Use		

STAGE 1 – APPRAISAL AND PRELIMINARY ASSESSMENT

Did the proposal have a pre-application meeting:	No
Is the notification wording appropriate:	Yes
Have all Entities been referred to?	Yes
NOD outline saved into Objective	Yes
MPRG Triggers: Click to view Triggers	Does the proposal trigger referral to the MPRG: Yes - if yes, specify which triggers (insert manually): <input type="text" value="30+ representations received"/>
Has a conflict of interest been declared?	No (Brad – had involvement in the EIS and planning stages of this project in previous role at Purdon Planning)
Do representors have the right to review in ACAT? (See Schedule 1 of the Act and Schedule 3 of the Regulation)	No

Is the proposal consistent with the relevant zone objectives: (s.120(a))	<input checked="" type="radio"/> Yes <input type="text"/> <input type="radio"/> No - Note: Please discuss with Senior Manager <input type="text"/>
Is the proposal considered suitable for the land on which it is to take place: (s.120(b)) Note: The assessment officer may also have regard to any DRP and Pre-DA community consultation	Yes - Based on relevant legislation and the TP assessment, the proposal is considered suitable for the land
What is the probable impact of the proposed development (including nature, extent and significance of	<input type="radio"/> No probable impacts identified that require amendment or refusal of the proposed development

probable environmental impacts): (s.120(g))	<p>OR</p> <p><input checked="" type="radio"/> Condition(s) of approval have been imposed to address probable impacts</p> <p>OR</p> <p><input type="radio"/> Impacts identified require amendment of the proposal</p> <p>OR</p> <p><input type="radio"/> Impacts identified require refusal of the proposal</p> <p>Comments:</p> <div style="border: 1px solid black; padding: 5px;"> <p>Noise fence required - see Noise Management Plan + Screen planting NW bound</p> </div>
Is a public land management plan in force over the land, (s.120(f))	<p>NO</p> <p>If Yes – Has the DA been referred to the Conservator? Choose an item. Additional referral requested Click or tap to enter a date.</p>

Notes for the assessment officers / Key issues identified:

[Click here to enter text.](#)

Have any issues been identified that would result in a refusal:	No
Conditions required from initial review of application or Territory Plan assessment:	<p>Conditions have been added to draft NoD</p> <div style="border: 1px solid black; height: 20px; width: 100%;"></div>

Completion

Stage 1 assessment has been completed.

The application is suitable for advancement (to the next assessment Stage).

The DA is recommended for refusal – Reasons for refusal has been added to the Draft NoD.

Other. To discuss

Email has been sent to the DA Coordinator – DATE - 09/05/2019

Assessment officer:	George Cilliers	09/05/2019
----------------------------	-----------------	------------

STAGE 2 – INFORMATION COLLECTION (ENTITIES – including EPSDD DA Leasing)

The application was not subject to any entity referrals.

OR

The DA was referred to the following Entities (or internal Sections) with the following response(s):

Entity	Did the entity support the application?	Assessment comments / Conditions required
The Conservator of Flora & Fauna	No	
Evo Energy - Electricity	Yes - with conditions	Attach statement to NoD
Evo Energy - Gas	Yes - with conditions	Attach statement to NoD
ICON Water	No	Can be conditioned not take effect. Attach "failed to comply" statement to NoD
TCCS	Yes - with conditions	Include to NoD
Heritage	Not Applicable	
NCA (National Capital)	Yes- without conditions	Advice only
CRA (City Renewal)	Not Applicable	
ESA (Emergency Services)	Not Applicable	
EPA (Environment Protection)	No	Not in current format – conditions and advice provided – can proceed with decision, but include to NoD
Tree Protection Unit	Yes - with conditions	Include to NoD
ACT Health	Not Applicable	
Education	Not Applicable	
General Leasing	Not Applicable	
Other:		
EPSDD DA Leasing	Yes	<input checked="" type="radio"/> DA leasing advice entered in Draft NoD <input type="radio"/> Additional info required <input type="radio"/> DA Amendment required for Leasing matters <input type="radio"/> Leasing manager requests further referral at stage 6

LRP/MPRG

Does any Entity advice received trigger referral to either the LRP or MPRG:	No
	if yes, which Entity: If yes; Email sent to the DA Coordinator to book the DA in for MPRG/ LRP Date Sent: Click or tap to enter a date.

Does any entity advice received mean the application must be refused:	No
Conditions required from entity advice:	Entered in draft NoD: Yes

Final

Has the entity advice received on this application been considered: (s.120(e))	Yes, see summary above.
An <i>alias</i> of advice from the Utility service providers and other relevant entities has been moved into the approved plans folder.	Yes
Is an environmental significance opinion in force & relevant: (s.120(c))	<input checked="" type="checkbox"/> No known ESO applies <input type="checkbox"/> An Environmental Significance Opinion (ESO) was given on Click or tap to enter a date. for (purpose/development details below) <input type="checkbox"/> The ESO was granted Choose an item. conditions. <input type="checkbox"/> The proposed development/works are minor in nature and are unlikely to cause a significant adverse environmental impact. or <input type="checkbox"/> The proposed development / works are consistent with the ESO.

STAGE 3 – ISSUES CONSIDERATION (REPRESENTATIONS)

No representations were made in respect of the application under section 156 of the Planning and Development Act 2007.

OR

Representation/s were made in respect of the application under section 156 of the Planning and Development Act 2007. All Representation have been read and considered, and a summary of key concerns / issues can be found below:

<p>Specify number of Representation/s received:</p> <p style="text-align: center;">59</p>	<p>NOTE: If 10 or more Reprs – refer to MPRG</p>
--------------------------------------------------------------------------------------------------	-----------------------------------------------------------------

Area of concern / issue	Representation comment - Territory Plan response
See separate <i>Summary and table of representations</i> and separate <i>Response to issues raised by representors</i> saved in objective	
Do any issues raised in representations require a further information request: (s.141 or 144)	<p>No</p> <p><input type="text"/></p> <p>Has the further information request been reviewed by the manager? Choose an item. Note: must be reviewed prior to being requested</p> <p>Has an email been sent to the DA Coordinator to request further information? Choose an item. Date request sent in e-development: Click or tap to enter a date. Date email request sent: Click or tap to enter a date.</p>
Was a site inspection undertaken in this stage:	No - Aerial imagery on ACTMapi deemed sufficient

Completion

<input type="radio"/> Stages 2 and 3 has been completed with no issues identified.	
<input checked="" type="radio"/> Stages 2 and 3 has been completed with the following issues noted: Issues raised by representations have been addressed in the draft NoD.	
<input type="radio"/> Stages 2 and 3 has identified issues that prevent further assessment, specifically (insert below)	
<input type="text"/>	
Email has been sent to the DA Coordinator to advance DA to the next stage – DATE Click or tap to enter a date.	
Assessment officer: George Cilliers	Date: 24/05/2019

STAGE 4 – TERRITORY PLAN ASSESSMENT

Proposed Use	Q1. Is the proposed use allowable in the applicable zone/s? Yes Q2. Is the proposal permitted under the Crown Lease? Yes
Territory Plan assessment	
Select relevant code: (click box) (Select more than one if required)	<input type="checkbox"/> Multi-unit Housing Development code <input type="checkbox"/> Single Dwelling Housing Development code <input type="checkbox"/> Residential Zone Development code <input type="checkbox"/> Non-Urban Zones Development code <input checked="" type="checkbox"/> Parking and Vehicular Access General code <input checked="" type="checkbox"/> Crime Prevention through Environmental Design General code <input type="checkbox"/> Estate Development code <input type="checkbox"/> Waterways Water Sensitive Urban Design General code <input type="checkbox"/> Access and Mobility General code <input type="checkbox"/> Signs General code <input type="checkbox"/> Lease variation General code Specify relevant Development Code/s: <div style="border: 1px solid black; padding: 2px;">Industrial Zones Development Code & Transport and Services Zone Development Code</div> Specify relevant Precinct Code/s: <div style="border: 1px solid black; padding: 2px;">Fyshwich Precinct Map and Code</div>

The assessing officer undertook an assessment of the proposal and found that the following KEY rules and criteria warrant further discussion. Note: Further issues may have been identified in a plan based assessment that have not necessarily been included in this or is of a significance that did not warrant particular discussion.

NOTE: Assessment Officers to pay particular attention to all mandatory rules

Code	Rule / Criteria	Assessment/Discussion
		See separate document DA Assessment – Territory Plan Code Requirements saved in objective

Further assessment documents, if any, can be found in the assessment file in Objective Choose an item.

Additional Information

Is further information required at this phase: (s.141 or s.144)	No <div style="border: 1px solid black; height: 20px; width: 100%;"></div> <p style="color: red; font-weight: bold;">Note: Further Information should not be requested at this stage if the DA is to be refused</p>
List items if required	

Stage 4 officer to complete Has further information been requested	Yes If yes, DATE 04/04/2019
Stage 4 officer to complete Has further information been received	Yes If yes, DATE 20/05/2019 If yes, DATE 21/05/2019

Was a site inspection undertaken in the assessment stage:	No - Aerial imagery on ACTMapi deemed sufficient
Have any issues been identified that would result in a refusal:	No
Conditions required from initial review of application Stage 1 or Territory Plan Stage 4 assessment:	Conditions have been added to draft NoD <div style="border: 1px solid black; padding: 2px;">Lighting conditions (CPTED assessment)</div>

All representations made about this application been considered in the assessment of this proposal: (s.120(d))	Yes, see assessment above (See separate <i>Summary and table of representations</i> and separate <i>Response to issues raised by representors</i> saved in objective)
----------------------------------------------------------------------------------------------------------------	-----------------------------------------------------------------------------------------------------------------------------------------------------------------------

Completion

Stage 4 assessment has been completed.

The application is suitable for advancement (to the next assessment Stage). Any recommended conditions and/ or advisory notes have been added to the Draft NoD

The DA is recommended for refusal – Reasons for refusal has been added to the Draft NoD.

Other. To discuss e.g. partial approval/ refusal

Email has been sent to the DA Coordinator – DATE - Click or tap to enter a date.

Assessment officer:	George Cilliers	24/05/2019
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STAGE 6 – RECOMMENDED DETERMINATION OF PROPOSAL

Was the DA referred to LRP or MPRG	Yes – the advice of the LRP/MPRG is noted and considered in making the decision	
Has the leasing manager requested to be referred during the decision stage (see stage 2 assessment)	No If yes, an email was sent to the DA Coordinator for DA Leasing referral: Date: Click or tap to enter a date.	
Has leasing advice been incorporated into the decision:	Choose an item.	
Has the Notice of Decision been finalised:	Recommended reasons for decision:	Choose an item.
	Recommended conditions:	Choose an item.
	Entity advice:	Choose an item.
	Representations:	Choose an item.
	Checked third party appeal rights:	Choose an item.
The application is to be:	<input type="radio"/> Approved <input type="radio"/> Approved with conditions <input type="radio"/> Partially Approved/ Refused. Details below: <input type="text"/> <input type="radio"/> Refused	
Has a conflict of interest been declared? - <i>If Yes, the proposal must be Peer Reviewed by Stage 6 officer (without potential conflict) and Signed/determined by a Senior manager</i>	Choose an item.	
Does the determining officer have the correct classification to make the decision? <i>Refer to Classification Matrix</i>	Choose an item.	

Peer review To be completed ONLY IF the delegate (determining officer) undertook another stage in the assessment.

Reviewing officer name:		Classification:		Date:	Click or tap to enter a date.
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Email sent to the DA Coordinator to advance DA to the next stage – DATE - Click or tap to enter a date.

Sign Off

Determination:	In my findings, I have considered the advice and recommendations stages 1-5 in this assessment document and determine that the application is to be: <input type="radio"/> Approved <input type="radio"/> Approved subject to conditions consistent with the above assessment <input type="radio"/> Refused consistent with the above assessment OTHER/COMMENTS:	Date: Click or tap to enter a date.
Determining officer & Delegate:	Name:	Date Click or tap to enter a date.

Post Determination

Can stamped plans be released with decision?

Yes - Stage 6 officer to Stamp Plans and email Customer Services to Dispatch

No - select below:

S165 Conditions required to be satisfied - Stage 6 to organise Stamped Plans once endorsed

Leasing conditions to be satisfied - Once satisfied Plans to be Stamped and dispatched by Stage 6

3rd party appeals - Set reminder date in calendar when plans can be Stamped and promptly dispatched

Other (specify below)

Representations – DA201835108 – Proposed rail freight terminal and freight transport facility

Summary of issues

1. Rail Freight Terminal (rail use) is prohibited in IZ2.
2. Waste facility and waste incinerator.
3. EIS process should be concluded prior to any other DA being determined.
4. Other options must be considered through a strategic planning process for the rail freight terminal.
5. Viability of the freight transport facility.
6. Objectives and suitability of the land.
7. Public land management plan.
8. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.
9. Documentation is insufficient and lessee details are incorrect.
10. Insufficient consultation has been undertaken.
11. All entity advice should be considered.
12. Noise impacts on surrounding development need to be considered, including noise generated from the railway.
13. Traffic impacts on the surrounding area, including cumulative and future impacts.
14. Safety and structural integrity around rail line during construction.
15. Relocation of utilities and all works should be at the proponent's expense (no government funding for private development).
16. Contamination has not been appropriately addressed.
17. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land).

Table of representations

1.		1. Traffic impacts on the surrounding area, including cumulative and future impacts.
2.		<ol style="list-style-type: none"> 1. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Emissions from additional traffic. • Insufficient access for heavy vehicles. • Road damage from additional heavy vehicles. 2. Noise impacts on surrounding development need to be considered, including noise generated from the railway.

3.		<ol style="list-style-type: none"> 1. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Access to main roads • Freight movement data is lacking 2. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Option on the fringe of Canberra • Feasibility and economic analysis • Containerisation not efficient 3. Viability of the freight transport facility/rail freight terminal. <ul style="list-style-type: none"> • Rail freight terminal is not large enough to operate efficiently or safely. • Compare other regional terminals • Container storage space 4. Noise impacts on surrounding development need to be considered, including noise generated from the railway. 5. An environmental assessment of the cumulative impacts (of all proposals) should be provided including social, economic and the environmental. 6. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). 7. Safety around rail line during construction. 8. Documentation is insufficient and lessee details are incorrect. <ul style="list-style-type: none"> • False or misleading statements in the application
4.		<ol style="list-style-type: none"> 1. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Major infrastructure requires consideration of all environmental impacts • Feasibility and long term analysis required • Must not exclude other such facilities • Inconsistent with government strategies • Encroachment into critical infrastructure 2. Viability of the freight transport facility/rail freight terminal. <ul style="list-style-type: none"> • The proponents experience is lacking
5.		<ol style="list-style-type: none"> 1. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Major infrastructure requires consideration of all environmental impacts • Hume is more appropriate • Not large enough to be considered small terminal (when compared to Belfield in Sydney) • Consideration for alternate location

		<ol style="list-style-type: none"> 2. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Road network not designed for freight movement • Access to main roads • Freight movement data is lacking 3. An environmental assessment of the cumulative impacts (of all proposals) should be provided including social, economic and the environmental.
6.		<ol style="list-style-type: none"> 1. Public land management plan. <ul style="list-style-type: none"> • NSW land management policy 2. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Major infrastructure requires consideration of all environmental impacts • Located within rail corridor 3. Rail Freight Terminal (rail use) is prohibited in IZ2 <ul style="list-style-type: none"> • Buffer of land impeding access • Access on IZ2 considered as rail use 4. Noise impacts on surrounding development need to be considered, including noise generated from the railway. <ul style="list-style-type: none"> • Noise from the railway has not been addressed in the noise report • Freight handling will contribute to noise • Noise mitigation on the rail side of the block is required but not possible • Proposal will increase two additional rail movements • During construction • Audible alarms/beepers • False/misleading statements in the noise report • No assurance that equipment will meet noise standards • Noise impact, including cumulative impacts, on existing and future areas 5. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Additional heavy trucks on Wiluna, Lithgow and Ipswich Streets • Awkward access to the site • Freight and truck movements on-site • Collision protection should be applied to the proposal (within 10-20m of the rail line) • Consider safety and operational integrity of the railway 6. Safety and structural integrity around rail line during construction.

		<ul style="list-style-type: none"> • Risk assessment not undertaken • De-stabilisation of rail infrastructure • Plans should be prepared by a structural engineer <p>7. Viability of the freight transport facility/rail freight terminal.</p> <ul style="list-style-type: none"> • Infrastructure maintained by government if not successful • The proponents experience is lacking • Freight terminal will need to be relocated <p>8. Documentation is insufficient and lessee details are incorrect.</p> <ul style="list-style-type: none"> • Empty containers have not been considered. <p>9. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land).</p> <p>10. Contamination has not been appropriately addressed.</p>
7.		<p>1. Traffic impacts on the surrounding area, including cumulative and future impacts.</p> <ul style="list-style-type: none"> • Additional heavy trucks on Wiluna, Lithgow and Ipswich Streets • Access to the site and surrounding blocks impacted <p>2. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.</p> <ul style="list-style-type: none"> • Traffic, noise, visual, hydrology and land use impacts <p>3. Noise impacts on surrounding development need to be considered, including noise generated from the railway.</p> <ul style="list-style-type: none"> • Increased rail movements/noise <p>4. Contamination has not been appropriately addressed.</p>
8.		<p>1. Waste facility and waste incinerator.</p> <ul style="list-style-type: none"> • Conflicted policy • Fire risk • Incompatible with surrounding development • Health and environment <p>2. Noise impacts on surrounding development need to be considered, including noise generated from the railway.</p> <p>3. Traffic impacts on the surrounding area, including cumulative and future impacts.</p> <ul style="list-style-type: none"> • Additional heavy trucks will compromise intersections at Newcastle/Ipswich Streets, Canberra Avenue/Ipswich Street and Canberra Avenue and Geelong Street • Constrained traffic access

9.		<ol style="list-style-type: none"> 1. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Financial implications • Assessment of risk to the government 2. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Alternative wider areas of the corridor • Alternatives should be considered in IZ1 3. Viability of the freight transport facility/rail freight terminal. <ul style="list-style-type: none"> • Size and frequency is not enough • Cost of trucks • Type of freight not viable
10.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. 2. Documentation is insufficient and lessee details are incorrect. 3. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Not consistent with ACT Planning Strategy • Alternative wider areas of the corridor • Alternatives should be considered in IZ1 • Strategic planning should consider all unleased land and best options for a terminal • Other companies provide all aspects of the operation (including transport) • Will exclude opportunities for surrounding businesses to use rail 4. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Freight transport facility needs to be local to support the facility without greatly increasing traffic 5. Viability of the freight transport facility/rail freight terminal. <ul style="list-style-type: none"> • Rail freight terminal is not large enough to operate efficiently or safely. • Compare other regional terminals • Containerisation not efficient • Feasibility study is required 6. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Noise from rail, construction and maintenance activities and increased traffic and air pollution. 7. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Corridor protection

		<ul style="list-style-type: none"> • No public benefits
11.		<ol style="list-style-type: none"> 1. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Road transport is more efficient • Land is not big enough for a viable terminal 2. Viability of the freight transport facility/rail freight terminal. <ul style="list-style-type: none"> • Feasibility study is required • Transporting via road and rail add to total freight costs and transit times 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • No public benefits (structures on unleased land) 4. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Road freight transport still required, greatly increasing traffic
12.		<ol style="list-style-type: none"> 1. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Should not allow private use of unleased land 2. Public land management plan. 3. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Access will cause congestion which will impact on surrounding business 4. Viability of the freight transport facility/rail freight terminal. <ul style="list-style-type: none"> • Abandoned hardstands will be a burden on TCCS if not successful • Road transport more efficient • No direct access to major roads • Cannot be used by multi-users • Terminal is too small 5. Other options must be considered through a strategic planning process for the rail freight terminal. <ul style="list-style-type: none"> • Does not provide an adequate freight terminal for the community • Encroachment into a strategic rail corridor should not be permitted. • Safety issues to undertake construction within the corridor • Options should be decided through extensive consultation, site selection surveys, feasibility studies and environmental and risk assessments
13.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. 2. Traffic impacts on the surrounding area, including cumulative and future impacts. 3. Viability of the freight transport facility. <ul style="list-style-type: none"> • Feasibility study is required

14.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. 2. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Additional heavy vehicles 3. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Impacts on health • Property value will decrease. • Reduced business 4. Other options must be considered and strategic planning is required for the rail freight terminal.
15.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Proper consultation required • Requires connections to local trunk roads • Hume is better option • Locate with existing logistics activities and the complementary industrial activities • Requires larger operations • Inconsistent with Railway Masterplan 2009 2. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Area/zone not suitable • amenity, traffic and noise impacts • Impact future developments • No public benefit
16.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Proper consultation required • Requires connections to local trunk roads • Hume is better option • Locate with existing logistics activities and the complementary industrial activities • Requires larger operations • Inconsistent with Railway Masterplan 2009 2. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Area/zone not suitable

		<ul style="list-style-type: none"> • amenity, traffic and noise impacts • Impact future developments • No public benefit
17.		<ol style="list-style-type: none"> 1. Documentation is insufficient and lessee details are incorrect. <ul style="list-style-type: none"> • Noise and traffic report out of date and inadequate • Form 4 invalid • Community unable to make informed judgement 2. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Strategic environmental assessment should have been required as major infrastructure • Policies and guidelines for planning terminal • Risk assessment • Feasibility and needs study/data 3. Safety around rail line during construction. 4. Public land management plan. 5. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Empty containers considered with traffic movements • Distribution facilities located • Impact on passenger train services 6. Waste facility and waste incinerator. 7. Viability of the freight transport facility. <ul style="list-style-type: none"> • How will the rail freight terminal accommodate new users • Freight on return journey? • Data to accommodate freight trains
18.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • No policies and guidelines for railways • Further investigations are required regarding protection of the rail network 2. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Proposals should have been considered as one EIS • Impacts on rail corridor 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • This development cannot exclude future expansion of the rail line.

		<ul style="list-style-type: none"> • Proposal is a permanent encroachment • Rail corridor (through Fyshwick) is the currently the only access to Canberra
19.		<ol style="list-style-type: none"> 1. Rail Freight Terminal (rail use) is prohibited in IZ2 <ul style="list-style-type: none"> • Land cannot cope with future transport needs and will significantly disrupt surrounding sites • Uses are not permitted on the land • Previous applications considered inappropriate • Access and egress for railway use 2. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Old documentation submitted referencing materials recovery facility • All proposals should be considered cumulatively 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Direct sale allowed to proceed with prohibited development • Application states that the uses are consistent with the current crown lease • Documentation for a licence not provided with this DA 4. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Size and all options for the terminal should be considered by government • Strategic planning would consider access and traffic flows, surrounding area, impact of encroachments on the rail corridor
20.		<ol style="list-style-type: none"> 1. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Should be assessed in the impact track
21.		<ol style="list-style-type: none"> 1. Insufficient consultation has been undertaken.
22.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Inappropriate location (retail and services business centre) • Impacts regarding noise, air quality and traffic. • Impact on future uses • Hazards including fires • Direct access to the railway not permitted in NSW
23.		<ol style="list-style-type: none"> 1. Insufficient consultation has been undertaken. 2. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • A strategic consultation process is required • Hume is a more appropriate location

		<ol style="list-style-type: none"> 3. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Impact on the existing railway line (passenger trains)
24.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Inappropriate location (retail and services business centre) • Impacts regarding noise, air quality and traffic.
25.		<ol style="list-style-type: none"> 1. Fully support all concerns raised by ISCCC and Griffith Narrubundah Community Association Inc.
26.		<ol style="list-style-type: none"> 1. Waste Transfer terminal will cause impacts. <ul style="list-style-type: none"> • Future EIS for waste incinerator • Previous fires have created impacts • Air quality guidelines should be considered • Future proposals should be considered • Proposal only feasible if all proposals are approved 2. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Cumulative impacts should be considered regarding fire hazard, air, traffic, noise and odour • No legislative requirement but cumulative assessment should be considered anyway 3. Rail Freight Terminal (rail use) is prohibited in IZ2 <ul style="list-style-type: none"> • Documentation not consistent in stating whether railway use • Submission in relation to DA201834656 provided as reference
27.		<ol style="list-style-type: none"> 1. Rail Freight Terminal (rail use) is prohibited in IZ2 <ul style="list-style-type: none"> • Land cannot cope with future transport needs and will significantly disrupt surrounding sites • Uses are not permitted on the land • Previous applications considered inappropriate • Access and egress for railway use 2. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Old documentation submitted referencing materials recovery facility • All proposals should be considered cumulatively 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Direct sale allowed to proceed with prohibited development • Application states that the uses are consistent with the current crown lease • Documentation for a licence not provided with this DA 4. Other options must be considered and strategic planning is required for the rail freight terminal.

		<ul style="list-style-type: none"> • Size and all options for the terminal should be considered by government • Strategic planning would consider access and traffic flows, surrounding area, impact of encroachments on the rail corridor <p>5. Documentation is insufficient and lessee details are incorrect.</p> <ul style="list-style-type: none"> • Letter of authorisation (TCCS) not provided • Noise management plan is insufficient
28.		<p>1. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land).</p> <ul style="list-style-type: none"> • No sub-licence issued and/or not provided as part of the application • Loading and unloading of the train should be on private land • Conditions of support by TfNSW require multi-user access to the rail line • False or misleading statements <p>2. Rail Freight Terminal (rail use) is prohibited in IZ2</p> <ul style="list-style-type: none"> • Section 50 of the Act prohibits decisions inconsistent with the Territory Plan
29.		<p>1. Contamination has not been appropriately addressed.</p> <ul style="list-style-type: none"> • Possible contamination on adjacent sites <p>2. Documentation is insufficient and lessee details are incorrect.</p> <ul style="list-style-type: none"> • Entity advice not provided (EPA) • Community at disadvantage and thorough representations cannot be provided • Section 189 of the Act should be invoked due to False or misleading statements • Contents and documentation recycled from previous DAs • TCCS authorisation (ACTPLA cannot proceed with application) • Authorisations and documentation have been used for multiple applications • False or misleading statements • Application indicated consistency with the crown lease • NSW Government funding is for upgrade of south shunt only • Application does not outline the proposal (cargo to be transported) <p>3. Rail Freight Terminal (rail use) is prohibited in IZ2</p> <ul style="list-style-type: none"> • Wrong track (should be impact track) • Access and egress to the Terminal is required across IZ2 • Consent for rail terminal provided from John Holland Rail • Previous applications determined that the proposal was prohibited • Size restrictions will require rail use in IZ2

		<ul style="list-style-type: none"> • False or misleading statements <ol style="list-style-type: none"> 4. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Government has undertaken steps that are in support of the facility • Community has not had access to assistance from the authority • Should take into account public interest, amenity and safety 5. Unleased land, a licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Deed of Licence will not allow a use other than a use in the Deed to any sub-Licence of the Deed • Consent from John Holland Rail is does not allow construction • False or misleading statements in relation to ownership of unleased land • The stated avenue for authorisation of the use on unleased land is confusing • Responsibility of land when CRS fail? 6. Objectives and suitability of the land. <ul style="list-style-type: none"> • Not consistent with the Zone objectives • The size and location of the land is unsuitable 7. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Splitting DAs lacks transparency • Safety issues with circulation 8. EIS process should be concluded prior to any other DA being determined. 9. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Access and egress • Planned extension of Lithgow Street with Geelong and Yallourn Streets will not continue • Increased traffic congestion and safety issues • Traffic report is insufficient 10. Noise impacts on surrounding development need to be considered, including noise generated from the railway. <ul style="list-style-type: none"> • Noise report is insufficient 11. Relocation of utilities and all works should be at the proponent's expense (no government funding for private development). 12. All entity advice should be considered. <ul style="list-style-type: none"> • EPA guidelines need to be considered 13. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.
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		<ul style="list-style-type: none"> • Representations raise impacts relating to zoning uses, increased traffic and safety issues, increased air pollution, business downturn, job losses and property devaluation
30.		<ol style="list-style-type: none"> 1. Viability of the freight transport facility. <ul style="list-style-type: none"> • Regular materials for transport has not been demonstrated • Previous transport of materials failed (using Kingston terminal) • Size of the site cannot cater for subsidiary facilities • Distance to main roads not viable • Benefit to the local economy has not been considered 2. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • E-commerce and internet shopping can remove trucks off the road
31.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. 2. Insufficient consultation has been undertaken. 3. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Community has not had opportunity to consider all impacts 4. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Location should be considered by government, in consultation with the community • Better located in IZ1 zone and/or Hume • Conflicts with the location in the Integrated Transport Strategy 2019-2045 (Canberra railway station)
32.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Not suitable for Fyshwick (light industry, retail and commercial enterprises) • Not wanted by the community • Small regional RFTs provide limited employment and economic growth 2. Viability of the freight transport facility. <ul style="list-style-type: none"> • Road transport is cheaper • Economies of scale will only be successful • Size of block not large enough and does not include necessary equipment for transporting containers • Rail use will have to spill on to IZ2 land (prohibited) • Small regional RFTs provide limited employment and economic growth 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Requirements of \$1m Transport for NSW grant • Should have public benefit.

		4. Waste facility and waste incinerator.
33.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Railway Masterplan 2009 and the Rail Freight Strategy 2016 should be considered • Master plan suggested intermodal facility to reduce the need for long haul road transport 2. Objectives and suitability of the land. <ul style="list-style-type: none"> • Zoning is unsuitable because of impacts on amenity, traffic, noise etc 3. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.
34.		<ol style="list-style-type: none"> 1. Viability of the freight transport facility. <ul style="list-style-type: none"> • Size of block not large enough and does not include necessary equipment for transporting containers • Width of platform is not big enough 2. Objectives and suitability of the land. <ul style="list-style-type: none"> • Unsafe within the rail line • Inconsistent with zone objectives • developments is incompatible with existing industries 3. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Increase in heavy vehicles, congestion, pollution and compromise safety • Poor access to the site and major roads • Existing traffic impacts 4. Noise impacts on surrounding development need to be considered, including noise generated from the railway. 5. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Not suitable in this location • Community consultation not undertaken 6. Waste facility and waste incinerator. 7. Rail Freight Terminal (rail use) is prohibited in IZ2.
35.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Should be consistent with the Territory Plans Statement of Strategic Direction (noise and traffic impacts and no employment growth or diversified economy, does not provide a transport hub and not connected to public transport) • Does not align with the Planning Strategy (not suitable for zone or area) • Further planning required along with community consultation

		<ul style="list-style-type: none"> • Should be located near greenfield area or Hume • Inconsistent with Statement of Planning Intent (not foster economic and employment hubs, inhibit the rail corridor) • West Fyshwick located around 800m from site <ol style="list-style-type: none"> 2. Waste facility and waste incinerator. <ul style="list-style-type: none"> • West Fyshwick located around 800m from site 3. Objectives and suitability of the land. <ul style="list-style-type: none"> • Objective I) should apply
36.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Not suitable for Fyshwick (light industry, retail and commercial enterprises) • Not wanted by the community • Small regional RFTs provide limited employment and economic growth • Community consultation not undertaken 2. Viability of the freight transport facility. <ul style="list-style-type: none"> • Road transport is cheaper • Economies of scale will only be successful • Size of block not large enough and does not include necessary equipment for transporting containers • Rail use will have to spill on to IZ2 land (prohibited) • Small regional RFTs provide limited employment and economic growth • Width of platform is not big enough 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Requirements of \$1m Transport for NSW grant • Should have public benefit. 4. Waste facility and waste incinerator. 5. Objectives and suitability of the land. <ul style="list-style-type: none"> • Unsafe within the rail line • Inconsistent with zone objectives • developments is incompatible with existing industries 6. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Increase in heavy vehicles, congestion, pollution and compromise safety • Poor access to the site and major roads • Existing traffic impacts

		<p>7. Noise impacts on surrounding development need to be considered, including noise generated from the railway.</p> <p>8. Rail Freight Terminal (rail use) is prohibited in IZ2.</p>
37.		<p>1. Noise impacts on surrounding development need to be considered, including noise generated from the railway.</p> <p>2. Traffic impacts on the surrounding area, including cumulative and future impacts.</p> <ul style="list-style-type: none"> • Increase in heavy vehicles, congestion, pollution and compromised safety • Obstructed road access, reduced parking and additional heavy vehicles would discourage residents using other business • Truck turning have to on the other side of the road and therefore will prevent Lithgow street extension • Encroachment into rail corridor compromises safety • impacts of safety and access <p>3. Other options must be considered and strategic planning is required for the rail freight terminal.</p> <ul style="list-style-type: none"> • No community engagement • Strategic rail corridor should not be used privately <p>4. Waste facility and waste incinerator.</p> <ul style="list-style-type: none"> • Fires and air quality issues at the facility • Veolia Woodlawn has been fined (by NSW EPA) for air pollution, leakage of leachate from rail containers • Mugga is more efficient and properly managed/regulated • Future waste transfer shed is large enough to accommodate a future Waste to Energy Incinerator <p>5. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.</p> <p>6. Viability of the freight transport facility.</p> <ul style="list-style-type: none"> • Previous freight facility has failed • Waste is the only freight that will be viable • Veolia Woodlawn will have the control over pricing <p>7. Rail Freight Terminal (rail use) is prohibited in IZ2.</p> <ul style="list-style-type: none"> • Surrounding commercial and retail businesses not compatible
38.		<p>1. Traffic impacts on the surrounding area, including cumulative and future impacts.</p> <ul style="list-style-type: none"> • Truck noise and air pollution <p>2. Waste facility and waste incinerator.</p>

		<ul style="list-style-type: none"> • Risk of fire <p>3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land).</p> <ul style="list-style-type: none"> • Other private facilities on unleased land (school developments) have been rejected • Development, such as relocation of utilities, is being partially subsidised (not in public interest)
39.		<p>1. Traffic impacts on the surrounding area, including cumulative and future impacts.</p> <ul style="list-style-type: none"> • Will impact on surrounding commercial and community precinct • Rail freight terminal will impact on amenity and traffic flows • Increase in heavy vehicles will impact on traffic flows and safety <p>2. Other options must be considered and strategic planning is required for the rail freight terminal.</p> <ul style="list-style-type: none"> • Consider another option such as a greenfield area • intermodal rail freight terminal to provide multi-user access and amenity into the future
40.		<p>1. Noise impacts on surrounding development need to be considered, including noise generated from the railway.</p> <ul style="list-style-type: none"> • Noise and vibration will impact on workers in Fyshwick • Rail operations, including loading/unloading, will can carry long distance • Insufficient space along the hardstand to mitigate against noise • Acoustic measures and noise impact assessment not sufficient (details of acoustic fence) • An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. • Noise impacts from all developments
41.		<p>1. Traffic impacts on the surrounding area, including cumulative and future impacts.</p> <ul style="list-style-type: none"> • Impact on traffic flows • Inconsistent with National Road Strategy (does not reduce heavy vehicles in urban and regional areas) • Heavy vehicles will increase serious accidents <p>2. Other options must be considered and strategic planning is required for the rail freight terminal.</p> <ul style="list-style-type: none"> • Hume is more appropriate (away from residential and commercial activity) and in better economic position with community support <p>3. Waste facility and waste incinerator.</p>
42.		<p>1. Traffic impacts on the surrounding area, including cumulative and future impacts.</p> <ul style="list-style-type: none"> • Cumulative impacts from all proposed developments • Impact from heavy vehicles u and increase congestion, pollution and compromise safety • Impact road access to local business

		<ul style="list-style-type: none"> • Increase in heavy vehicles will discourage residents from using local services • Businesses depend on access and on-street parking for their customers and some businesses may close <ol style="list-style-type: none"> 2. Viability of the freight transport facility. <ul style="list-style-type: none"> • One trip per week does not make the facility viable • Previous freight transport (Access Recycling) to Port Botany failed
43.		<ol style="list-style-type: none"> 1. Viability of the freight transport facility. <ul style="list-style-type: none"> • Insufficient freight without the waste facility • In 2015 a scrap metal transport facility failed 2. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Facility should be built on private land • Spur line on private land could be rebuilt • Rail corridor needs to be protected for future use
44.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Does not services the needs of Fyshwick • Limit future freight terminals • Rail terminal should not be considered in isolation (wider public and business needs, and regional and national network) • Freight strategy would be more acceptable to community and fewer environmental conflicts • Strategies would consider options and access 2. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). 3. Rail Freight Terminal (rail use) is prohibited in IZ2 <ul style="list-style-type: none"> • Access and egress for railway use
45.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Economic feasibility must be evaluated • Railway Masterplan 2009 and the Freight Strategy 2016 must be considered 2. Rail Freight Terminal (rail use) is prohibited in IZ2. 3. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Facility would add to traffic congestion • Parking will be disrupted and impact on surrounding businesses • Trucks cross the wrong side of the road at end of Lithgow Street • Increase heavy vehicle collisions • Impact on road safety and surrounding roads may need to be upgraded (who pays)

		<ol style="list-style-type: none"> 4. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Will cause traffic issues on Ipswich, Wiluna and Lithgow Streets
46.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Consultation with government is required to determine appropriate location • Previous government reports suggested IZ1 areas • Need for the development not demonstrated • Long term feasibility • Impacts on local residents and business 2. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Increase in heavy vehicles 3. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Increase in heavy vehicles 4. Viability of the freight transport facility. <ul style="list-style-type: none"> • Rail freight terminal is dependant on the waste facility to succeed • Previous scrap metal freight facility failed (2015)
47.		<ol style="list-style-type: none"> 1. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Truck noise and air pollution • Will impact on surrounding commercial and community precinct • Rail freight terminal will impact on amenity and traffic flows • Increase in heavy vehicles will impact on traffic flows, congestion, pollution and safety • Inconsistent with National Road Strategy (does not reduce heavy vehicles in urban and regional areas) • Parking will be disrupted and impact on surrounding businesses • Trucks cross the wrong side of the road at end of Lithgow Street • Surrounding roads may need to be upgraded (who pays) • Cumulative impacts from all proposed developments 2. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Risk of fire • Increase in heavy vehicles will cause traffic issues on Ipswich, Wiluna and Lithgow Streets 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Other private facilities on unleased land (school developments) have been rejected • Development, such as relocation of utilities, is being partially subsidised (not in public interest) • Facility should be built on private land

		<ul style="list-style-type: none"> • Spur line on private land could be rebuilt • Rail corridor needs to be protected for future use <ol style="list-style-type: none"> 4. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Consider another option such as a greenfield area • Intermodal rail freight terminal to provide multi-user access and amenity into the future • Hume is more appropriate (away from residential and commercial activity) and in better economic position with community support • Economic feasibility must be evaluated • Railway Masterplan 2009 and the Freight Strategy 2016 must be considered • Consultation with government is required to determine appropriate location • Previous government reports suggested IZ1 areas • Need for the development not demonstrated • Long term feasibility 5. Noise impacts on surrounding development need to be considered, including noise generated from the railway. <ul style="list-style-type: none"> • Noise and vibration will impact on workers in Fyshwick • Rail operations, including loading/unloading, will can carry long distance • Insufficient space along the hardstand to mitigate against noise • Acoustic measures and noise impact assessment not sufficient (details of acoustic fence) 6. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. <ul style="list-style-type: none"> • Noise impacts from all developments 7. Viability of the freight transport facility. <ul style="list-style-type: none"> • Insufficient freight without the waste facility • In 2015 a scrap metal transport facility failed • One trip per week does not make the facility viable 8. Rail Freight Terminal (rail use) is prohibited in IZ2.
48.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Spread of odour to surrounding uses • Will give monopoly of waste management to CRS • Community will bear economic costs of the waste facility • Private benefits only

		<ol style="list-style-type: none"> 2. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • No benefit to the ACT community and government 3. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Increased traffic congestion on southern approach 4. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Future upgrades or growth to the rail corridor will be precluded (terminal is encroaching) • Not compatible with the markets precinct and other more suitable options in Hume 5. Documentation is insufficient. <ul style="list-style-type: none"> • Letter of authorisation (TCCS) 6. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.
49.		<ol style="list-style-type: none"> 1. Viability of the freight transport facility. <ul style="list-style-type: none"> • CRS have not demonstrated experience or skills to operate the facility • Application states only one employee to manage the site • Management of the site has not been demonstrated (i.e. arrivals/departures, loading/unloading etc.) • Not compatible with surrounding developments (freight travels further) • Rail is more expensive • Quoted freight loads over exaggerated • In 2015 a scrap metal transport facility failed • Government needs to evaluate the viability • Short-haul only works when co-locate with compatible facilities • Transporting empty containers is costly • The feasibility study in the Freight Strategy 2016 and Railway Masterplan must be completed 2. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Dangerously occupy the rail corridor
50.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Fyshwick is inappropriate (light industrial and retail services) • Issues of air pollution, odour, noise, traffic congestion and air safety • To close to where people live and work, children go to childcare and school • Government, industry and the community should determine a more suitable location, away from residential areas, like Hume
51.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator.

		<ul style="list-style-type: none"> • Conflicted policy • Fire risk • Incompatible with surrounding development • Health and environment <ol style="list-style-type: none"> 2. Noise impacts on surrounding development need to be considered, including noise generated from the railway. 3. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Additional heavy trucks will compromise intersections at Newcastle/Ipswich Streets, Canberra Avenue/Ipswich Street and Canberra Avenue and Geelong Street • Constrained traffic access
52.		<ol style="list-style-type: none"> 1. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Increased number of heavy vehicles • Traffic limitations of the current Ipswich Street, Newcastle Street and Monaro Highway • Upgrades to the area are required (Extend dual lane off ramp Dairy Road/Monaro, remodel signal phasing/timing and ease of movement with circulation, merging and additional lanes) 2. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Traffic report for the materials recovery facility was reviewed by Calibre Consulting, comments below • The proposed signalised lights on Ipswich Street are likely to be detrimental to northbound traffic due to delays • Different access arrangement will reduce conflicts at the access on Ipswich street, helping to improve safety • Existing traffic impacts in Fyshwick • Previous traffic studies predict, as early as 2026, the level of service of the intersections in the area and the Monaro Highway itself will fall to unacceptable levels (without road improvement)
53.		<ol style="list-style-type: none"> 1. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • traffic assessment must consider traffic movement from Holcim (26 Lithgow Street, Fyshwick) • Site entrance should be constructed as per plans • A no stopping zone should be implemented on the northern side of Lithgow Street (for continued flow of traffic) • Operation to avoid heavy vehicles staging • Current safety issues at four Way Intersection (Canberra Ave and Geelong Street), additional traffic lights might be required

54.		<ol style="list-style-type: none"> 1. Rail Freight Terminal (rail use) is prohibited in IZ2 <ul style="list-style-type: none"> • Fyshwick includes retail, commercial and entertainment activities (incompatible with rail) • Access and egress for rail use across IZ2 2. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Future expansion needs to be considered 3. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Proposal encroaches into the rail corridor • Construction within the rail corridor is dangerous 4. Relocation of utilities and all works should be at the proponent's expense (no government funding for private development). 5. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.
55.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Incompatible with Fyshwick • Noise and traffic impacts • Heavy vehicles increase congestion, pollution and compromise safety • Adverse impact on local businesses (increase in parking and traffic) 2. Rail Freight Terminal (rail use) is prohibited in IZ2 <ul style="list-style-type: none"> • Narrow corridor and other blocks show spur lines on private property • Access across other blocks on IZ2 3. Viability of the freight transport facility. <ul style="list-style-type: none"> • No access to main sites
56.		<ol style="list-style-type: none"> 1. An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental. 2. Viability of the freight transport facility. <ul style="list-style-type: none"> • 50-60 containers not enough as viable business model 3. Documentation is insufficient. <ul style="list-style-type: none"> • No details about buildings, site movements, amenities, machinery/equipment/tools/fuel etc. • Documentation does not demonstrate how the facility will function separately • DA201834656 was referenced but was not incorporated into this application 4. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Another site (other than Fyshwick) could be used

		<ol style="list-style-type: none"> 5. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Unsafe exit arrangements 6. Waste facility and waste incinerator.
57.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Too close to residences in Narrabundah, Griffith, the Kingston Foreshore and Oaks Estate, many schools, shops and offices
58.		<ol style="list-style-type: none"> 1. Waste facility and waste incinerator. <ul style="list-style-type: none"> • Consider cumulative social, environmental and economic matters. • No public benefit and not in public interest • Should be in broadacre zone • Traffic impacts, reduced air quality (odour), risks of accident and fire, risks to water quality and risk to air safety at Canberra International Airport among others • Other options must be considered and strategic planning is required for the rail freight terminal • No climate change consideration • Cost benefit analysis should have been included • Consideration of ACT waste and future • Not compatible with the surrounding area • Size of block inadequate and not suitable for the facility • Noise impacts, hazardous waste and increased litter • Leachate and stormwater management
59.		<ol style="list-style-type: none"> 1. Other options must be considered and strategic planning is required for the rail freight terminal. <ul style="list-style-type: none"> • Protect the rail corridor for future upgrades 2. A licence or lease has not been granted for Block 11 Section 8 Fyshwick (unleased land). <ul style="list-style-type: none"> • Deed of licence 2017 does not allow for construction • Unloading and loading must be on private land • The use of public land for a long term facility is not good enough 3. Traffic impacts on the surrounding area, including cumulative and future impacts. <ul style="list-style-type: none"> • Increased heavy vehicles 4. Objectives and suitability of the land. <ul style="list-style-type: none"> • Not compatible with surrounding development

PUBLIC NOTIFICATION

The development application was originally publicly notified from 7 March 2019 to 28 March 2019. However, an error was identified early in the notification process and therefore this application was renotified. Pursuant to Division 7.3.4 of the Act, the application was publicly notified from 12 March 2019 to 1 April 2019. Fifty nine written representations were received during public notification. The main issues raised were as follows. Comments are provided as appropriate.

(a) Rail Freight Terminal (rail use) is prohibited in IZ2.

This development application (DA) was submitted for the construction of a rail freight terminal on Block 11 Section 47 Fyshwick and freight transport facility on Blocks 9 and 11 Section 8 Fyshwick. The proposal is predominantly for loading, unloading and manoeuvring shipping containers to be transported by rail from Block 47.

The proposed uses are listed as assessable uses in the TSZ2 Services zone and IZ2 Mixed Use Industrial Zone development tables, Territory Plan. Therefore, the application was accepted in the merit track and assessed in accordance with the requirements of the *Planning and Development Act 2007*.

(b) Waste facility and waste incinerator.

This DA does not include a waste facility or a waste incinerator. The applicant has lodged an application for a materials recovery facility (MRF) which is currently being considered by the Authority through the Environmental Impact Statement (EIS) process (EIS201700053). The EIS process has not yet been finalised and the authority is considering all impacts of the proposal including the cumulative impacts.

Prior to this application and the EIS, the applicant proposed a waste to energy facility and MRF on the subject site. The waste to energy facility was proposed under a separate EIS process (EIS201700023). On 30 June 2018 a Scoping Document was issued for EIS201700023 and the EIS did not progress any further. The Scoping Document has now lapsed.

(c) EIS process should be concluded prior to any other DA being determined.

This development application was lodged in the merit track. A completed EIS is not required for this application.

(d) Other options must be considered through a strategic planning process for the rail

The Authority is required to assess and determine the application based on the option the applicant has proposed. The proposal has been assessed, as submitted, in accordance with the requirements of the Territory Plan and the *Planning and Development Act 2007*.

(e) Viability of the freight transport facility.

The commercial viability of the proposal is not a consideration under the Territory Plan or for this development application. The proponent is responsible for investigating the economic viability of their proposed activity.

(f) Objectives and suitability of the land.

An assessment of the proposal has been undertaken and consideration was given to the zone objectives and suitability of the land and determined that the proposal is suitable in this location. The subject site has previously been used as a fuel storage and fuel transportation to and from the site, using rail and trucks. The site has now ceased operation and the land has been vacated for a number of years. The proposed development is intending to improve the existing infrastructure and use the site as a freight transport facility and adjacent rail freight facility which will provide increased investment and employment opportunities within Fyshwick. The proposed facility is of a nature that is unlikely to adversely impact on the surrounding industrial activities and is compatible with the existing and possible future surrounding uses and the proposed works is adaptable to suit any future use on the site. The proposal intends to make use of and improve the existing rail network to provide a directly accessible rail freight terminal that is positioned for quick loading/unloading of goods.

(g) Public land management plan.

An area of public land is identified as an overlay in the Territory Plan Map. These overlays include areas such as a national park, a special purpose reserve or a lake. All areas identified with a public land overlay must be managed in accordance with the management objectives applying to the area and any relevant plan of management.

Although the proposal includes areas of unleased land, there are no public land overlays identified for this site.

(h) An environmental assessment of the cumulative impacts (of all proposals) should be undertaken/provided including social, economic and the environmental.

The probable impacts of the proposed development, including the cumulative impacts of other developments within the area, have been considered as part of the assessment. The location, nature and scale of this application were considered and determined that the proposal is unlikely to cause significant environmental impacts on the subject site and/or on the surrounding area. Any other application will be assessed on a case by case basis, by the Authority, as each application is made.

(i) Documentation is insufficient and lessee details are incorrect.

On submission, this development application was checked by Access Canberra and a number of sections in EPSDD to ensure enough information was provided for public notification, entities and assessment purposes. During the assessment, further information was requested and submitted under s 141 of the *Planning and Development Act 2007* to clarify parts of the application, including authorisation, utilities and traffic and flooding impacts.

The further information was considered by relevant entities and the Authority and was considered sufficient to make a decision on the proposal.

(j) Insufficient consultation has been undertaken.

Pre-DA community consultation is required for developments that are for:

- *a building for residential use with 3 or more storeys and 15 or more dwellings*
- *a building with a gross floor area of more than 5000m²*
- *buildings that have a total gross floor area of more than 7000m²*
- *a building or structure more than 25m above finished ground level*
- *a variation of a lease to remove its concessional status.*

The proposed development does not activate any of the above triggers for community consultation. Consultation undertaken prior to a DA is at the proponent's discretion.

(k) All entity advice should be considered.

This development application was referred to each mandatory entity as part of the application process. During the assessment, clarification was requested by the Authority under s 141 of the Act relating to authorisation, utilities, noise, traffic and flooding. The applicant provided further detail that was then endorsed by relevant entities. This decision is not inconsistent with advice received from mandatory entities.

(l) Noise impacts on surrounding development need to be considered, including noise generated from the railway.

Under the Territory Plan, a noise Management Plan is required for certain uses to ensure noise impacts are mitigated in relation to other incompatible uses. The proposed development is not identified in the Territory Plan as a use requiring a noise management plan. Nevertheless, a noise management plan was submitted as part of the application and referred to relevant agencies. The authority and agencies determined that the proposed mitigation measures and the noise produced by the proposed activity would be at an acceptable level for the industrial zone.

Conditions of approval were incorporated into this decision to ensure noise management measures are adopted.

(m) Traffic impacts on the surrounding area, including cumulative and future impacts.

Traffic was a consideration during the assessment process. Specifically, the authority considered whether an efficient and safe access/egress arrangement was proposed and also considered whether the loading of vehicles are likely to cause unreasonable traffic impacts. Further detail was requested during the assessment process, relating to site access and visibility, vehicle turning and circulation and traffic movements.

The Authority concluded that the proposal is unlikely to adversely impact on the area and/or the safety of users.

In addition, the application was referred to Transport Canberra and City Services (TCCS) who endorsed the application.

(n) Safety and structural integrity around rail line during construction.

All works must be undertaken in accordance with a temporary traffic management plan (TTMP), approved by TCCS. The TTMP is to ensure all works are undertaken safely with minimal disruption to the public transportation networks. Conditions of approval have been included in this decision to ensure TCCS requirements are met.

In addition, works on unleased land is managed by TCCS in accordance with the *Public Unleased Land Act 2013*. Under the Act, TCCS can issue directions to repair unleased land and enforce any non-compliance. The matters raised are managed by TCCS as works is undertaken.

(o) Relocation of utilities and all works should be at the proponent's expense (no government funding for private development).

Financial negotiations relating to the cost of utilities is not a consideration by the Authority for this application.

(p) Contamination has not been appropriately addressed.

This application was referred to the Environment Protection Authority (EPA) in accordance with the requirements of the *Planning and Development Act 2007*. On 25 March 2018 advice was received from EPA who raised concerns relating to contamination. EPA's advice has been incorporated as conditions of approval.

(q) A licence or lease has not been granted for Block 11 Section 8 Fyshwick and Block 11 Section 47 Fyshwick (unleased land).

An application that includes works on unleased land needs to be authorised by the Government Land Custodian. Authorisation from the government land custodian (TCCS) was submitted under s 141 of the Act during the assessment process. The application was referred to the custodian to consider all matters relevant to the entity including how the land will be used or managed by the proponent. TCCS provided advice endorsing the proposal, with conditions. A condition of approval has been imposed that requires the proponent to obtain a licence or lease to use the unleased land, prior to construction. TCCS advice has been incorporated as conditions of approval.

From: [Cilliers, George](#)
To: [ISCCC Chairperson](#)
Cc: [Rutledge, Geoffrey](#); [Marcantonio, Laura](#); [McRae, Poppy](#); [Ponton, Ben](#); [Phillips, Brett](#)
Subject: RE: Request for extension of closing date for representations on DA201835108-Proposal for Rail Freight Terminal, Freight Transport Facility and Access in Fyshwick [SEC=UNCLASSIFIED]
Date: Monday, 25 March 2019 5:03:34 PM

Good afternoon Ms Fatseas,

I'm responding on behalf of Mr Phillips, and as a delegate of the Chief Planning Executive.

I've considered your request, including the four reasons you've forwarded to support your request, but unfortunately cannot agree to an extension of time for the purpose of making representations.

In considering your request, I've also noted that: DA201835108 was lodged on 30 January 2019. The original notification period was 7 March 2019 to 28 March 2019. However, an administrative error was identified early in the notification process and the application was renotified. The DA was renotified from 12 March 2019 to 1 April 2019, for 15 working days, in accordance with requirements of the *Planning and Development Act 2007*. The statutory timeframe to make a decision on a development application with representations is 45 working days. The legal due date for making a decision for DA201835108 is therefore 4 April 2019.

Given that an extension to the public notification period would exceed the legal due date for making a decision it is not possible to agree to such an extension.

However, please be assured that all representations received within the public notification timeframe will be considered by the planning and land authority prior to making a decision.

Regards

George Cilliers | Executive Branch Manager, Development Assessment

Phone 02 6207 6804

Planning Delivery | Environment, Planning and Sustainable Development | **ACT Government**

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From: Ponton, Ben

Sent: Wednesday, 20 March 2019 12:37 PM

To: ISCCC Chairperson [REDACTED] Phillips, Brett <Brett.Phillips@act.gov.au>

Cc: Rutledge, Geoffrey <Geoffrey.Rutledge@act.gov.au>; Marcantonio, Laura <Laura.Marcantonio@act.gov.au>; McRae, Poppy <Poppy.McRae@act.gov.au>; Cilliers, George <George.Cilliers@act.gov.au>

Subject: RE: Request for extension of closing date for representations on DA201835108-Proposal for Rail Freight Terminal, Freight Transport Facility and Access in Fyshwick

UNCLASSIFIED

Dear Ms Fatseas – as this is a statutory decision for the planning and land authority I have not included the Members of the Legislative Assembly, copied into your original e-mail, into this response.

It is important to the integrity of the planning system, that we keep statutory decisions of the planning and land authority separate from elected members, as required by the *Planning and Development Act 2007*. We'll of course provide advice to the Minister, for information, once we've made a decision on your request.

I have asked Mr Brett Phillips, as my delegate, to consider this request and Mr Phillips will be in contact very soon.

Yours sincerely,

B

Ben Ponton | Director-General

Phone +61 2 6207 8359

Environment, Planning and Sustainable Development Directorate | **ACT Government**

Level 3, 16 Challis Street, Dickson | GPO Box 158 Canberra ACT 2602 | www.environment.act.gov.au

From: ISCCC Chairperson [REDACTED]

Sent: Wednesday, 20 March 2019 11:37 AM

To: Ponton, Ben <Ben.Ponton@act.gov.au>; Phillips, Brett <Brett.Phillips@act.gov.au>

Cc: Gentleman, Mick <Mick.Gentleman@act.gov.au>; RATTENBURY <RATTENBURY@act.gov.au>; CANDICE <CANDICE@parliament.act.gov.au>; Lee, Elizabeth <Elizabeth.Lee@parliament.act.gov.au>; BARR Reception <BARR@act.gov.au>; STEPHEN-SMITH <STEPHEN-SMITH@act.gov.au>; PARTON <PARTON@parliament.act.gov.au>

Subject: Request for extension of closing date for representations on DA201835108-Proposal for Rail Freight Terminal, Freight Transport Facility and Access in Fyshwick

FYSHWICK

Development Application: 201835108

Address: 16 IPSWICH STREET

Block: 11,9&11 **Section:** 47,8

Proposal: PROPOSAL FOR RAIL FREIGHT TERMINAL, FREIGHT TRANSPORT FACILITY AND ACCESS - construction and use of a rail freight terminal (on block 11 section 47) and freight transport facility (all blocks), as well as access driveways and associated works. This proposal includes Block 11 Section 47 and Blocks 9 and 11 Section 8.

Period for representations closes: 01/04/2019

[Click here to view the plans](#)

[Click here to lodge a representation](#)

Dear Mr Ponton

The ISCCC requests an extension of time for all public representations in response to DA201835108. The DA was not properly notified for the public to access until 19 March which should take the closing date from 1 April to 8 April. ISCCC requests a further 10 days for representations from 8 April until just after Easter on 23 April. We do so for the following reasons:

1. DA201835108 was not publicly available from the EPSDD website for five days unless the DA number, the relevant District being Canberra Central or the closing date for representations was known to the person wishing to make representations. It could not be located for five days in the usual manner of clicking on "F" for Fyshwick. The digital link was only provided to Fyshwick property owners near the proposed development.
2. DA201835108 for heavy rail infrastructure is interdependent with the major waste facility of EIS201700053 proposed for Ipswich St in Fyshwick. No pre-DA consultation has been undertaken in regard to the intermodal rail freight terminal by CRS or government. There has been overwhelming public opposition in community meetings and representations in response to the draft EIS about construction of an IZ1-General industry operation on Ipswich Street Fyshwick within the IZ2-Mixed use industrial surrounding. This is a significant development proposal requiring research of many documents and reports. Members of the community await FOI documents due in April and substantive TCCS response to communication regarding clarification of government policy and authority in this matter. The DA closing date is currently 1 April.
3. Closing for representations on 31 March is the ACT Government's community consultation on *Moving Canberra: An Integrated Transport Strategy*, also requiring significant time and effort on the part of ISCCC members and the community more broadly. It is a Strategy which encompasses all of Canberra's transport options until 2045. It is overburdening the community to have both complex documents open for representations with an almost identical deadline.
4. As reported in the Canberra Times on 16 March, the Authority will consider carefully the Supreme Court Decision (2019) ACT SC58. The Supreme Court Decision may have far reaching adverse consequences for residents, traders and motorists in inner south Canberra. We need time to reflect on this decision, and take its implications into account in responding to DA201835108.

The ISCCC looks forward to confirmation that an extension of time will be granted until just after Easter on 23 April to respond to DA201835108.

Kind regards

Marea Fatseas
Chair
Inner South Canberra Community Council

From: [Phillips, Brett](#)
To: [Bennett, JamesP](#)
Subject: FW: Request for extension of time to make representations to DA201835108 [SEC=UNCLASSIFIED]
Date: Tuesday, 3 December 2019 2:12:59 PM

UNCLASSIFIED

Brett Phillips | Executive Group Manager, Planning Delivery
Environment, Planning and Sustainable Development Directorate
Ph: 02 62073520 E: brett.phillips@act.gov.au
ACT Government Level 1, 16 Challis Street, Dickson | GPO Box 158 Canberra ACT 2602
www.environment.act.gov.au

From: Cilliers, George <George.Cilliers@act.gov.au>
Sent: Monday, 25 March 2019 5:04 PM
To: [REDACTED]
Cc: Rutledge, Geoffrey <Geoffrey.Rutledge@act.gov.au>; Ponton, Ben <Ben.Ponton@act.gov.au>; Phillips, Brett <Brett.Phillips@act.gov.au>; 'ISCCC' <info@isccc.org.au>; [REDACTED]; [REDACTED]; Marcantonio, Laura <Laura.Marcantonio@act.gov.au>; McRae, Poppy <Poppy.McRae@act.gov.au>; Vest, Petra <Petra.Vest@act.gov.au>
Subject: RE: Request for extension of time to make representations to DA201835108 [SEC=UNCLASSIFIED]

Good afternoon [REDACTED],

I've considered your request, but unfortunately cannot agree to an extension of time for the purpose of making representations.

In considering your request, I've also noted that: DA201835108 was lodged on 30 January 2019. The original notification period was 7 March 2019 to 28 March 2019. However, an administrative error was identified early in the notification process and the application was renotified. The DA was renotified from 12 March 2019 to 1 April 2019, for 15 working days, in accordance with requirements of the *Planning and Development Act 2007*. The statutory timeframe to make a decision on a development application with representations is 45 working days. The legal due date for making a decision for DA201835108 is therefore 4 April 2019.

Given that an extension to the public notification period would exceed the legal due date for making a decision it is not possible to agree to such an extension.

However, please be assured that all representations received within the public notification timeframe will be considered by the planning and land authority prior to making a decision.

Regards

George Cilliers | Executive Branch Manager, Development Assessment

Phone 02 6207 6804

Planning Delivery | Environment, Planning and Sustainable Development | **ACT Government**

Dame Pattie Menzies House, Challis Street, Dickson | GPO Box 1908 Canberra ACT 2601 | www.actpla.act.gov.au

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From: Ponton, Ben

Sent: Saturday, 23 March 2019 4:59 PM

To: [REDACTED] Cilliers, George

<George.Cilliers@act.gov.au>

Cc: Rutledge, Geoffrey <Geoffrey.Rutledge@act.gov.au>; Phillips, Brett

<Brett.Phillips@act.gov.au>; 'ISCCC' <info@isccc.org.au>; [REDACTED]

[REDACTED]; Marcantonio,

Laura <Laura.Marcantonio@act.gov.au>; McRae, Poppy <Poppy.McRae@act.gov.au>; Vest,

Petra <Petra.Vest@act.gov.au>

Subject: RE: Request for extension of time to make representations to DA201835108

UNCLASSIFIED

Dear [REDACTED] – thank you for your e-mail. I've referred this request to George Cilliers, Executive Branch Manager, Planning Delivery. Mr Cilliers will be in contact shortly.

Yours sincerely,

B

Ben Ponton | Director-General

Phone +61 2 6207 8359

Environment, Planning and Sustainable Development Directorate | **ACT Government**

Level 3, 16 Challis Street, Dickson | GPO Box 158 Canberra ACT 2602 | www.environment.act.gov.au

From: [REDACTED]

Sent: Friday, 22 March 2019 7:17 PM

To: Ponton, Ben <Ben.Ponton@act.gov.au>

Cc: Rutledge, Geoffrey <Geoffrey.Rutledge@act.gov.au>; Phillips, Brett

<Brett.Phillips@act.gov.au>; Cilliers, George <George.Cilliers@act.gov.au>; 'ISCCC'

<info@isccc.org.au>; [REDACTED]

Subject: Request for extension of time to make representations to DA201835108

Hi Ben

This is a request for an extension of the time to make public representations to DA201835108, the CRS proposal for a Rail Freight Terminal and a Freight Transport Facility at 16 Ipswich Street Fyshwick. Representation closes on 1 April 2018. The day earlier representation closes on the major TCCS Have Your Say *Moving Canberra: An Integrated Transport Strategy 2019 – 2045* which magically on page 33 has popped Fyshwick into the location for an intermodal freight terminal.

Additionally, there is the most disturbing little 6 Pirie Street Fyshwick amendment to an application which is no longer available on your website. The original proposal was for a lease variation which sought to introduce an IZ1 use being Hazardous Industry in order to upgrade the lease purpose in an IZ2 area. Now, without access to DA201834784 for reference, there is the amendment not available except via the zipped file. **Under List Amendments Being Satisfied** advises that the applicant, Purdon Planning is seeking: Updated Crown Lease wording to remove hasardous [sic] industry and noxious trade.

This is the message that appears when trying to open the two documents notified under *Click here to view the plans* below.

404 - File or directory not found.

The resource you are looking for might have been removed, had its name changed, or is temporarily unavailable.

FYSHWICK

Development Application: 201834784

Address: 6 PIRIE STREET

Block: 17 **Section:** 20

Proposal: AMENDMENT TO DA201834784 - Lease Variation. Amendment to development application for lease variation which is still under consideration - The amendment is to remove references to hazardous industry and noxious trade.

Period for representations closes: 28/03/2019

[Click here to view the plans](#)

[Click here to lodge a representation](#)

It doesn't take a brain surgeon to realise that should the term "noxious trade" be removed from a Crown Lease the implications of setting such a precedent for all IZ2-Mixed use industrial areas is established. All via a neat cheap Lease Variation testing the waters for substantial developments as proposed at 16 Ipswich Street. Is this more of ACTPLA partiality to developers or just carelessness?

There have been issues with website access to all Fyshwick DAs where for a week the usual link of "F" for Fyshwick did not function until last Tuesday following advice from the community. We also are waiting on highly relevant FOI documents in particular those promised by TCCS. It is a mystery what consent has already been provided by the land custodian TCCS to CRS.

Should you require further information in order to inform the veracity of this request for an extension please let me know. On the related CRS DA the ISCCC has requested an extension to 28 April.

Your earliest attention to this matter would be appreciated.

Respectfully yours

A solid grey rectangular box used to redact the signature of the sender.